



West Auburn Boulevard Special Planning Area Ordinance and Streetscape Master Plan

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West Auburn Boulevard Special Planning Area Ordinance and Streetscape Master Plan



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Executive Summary

The West Auburn Boulevard Special Planning Area Ordinance and Streetscape Master Plan are intended to guide land use and development along West Auburn Boulevard in a manner that will strengthen the Boulevard's market position as a positive business address. The Special Planning Area Ordinance offers specific recommendations to encourage private development. The Streetscape Master Plan portion of this document focuses on the public right-of-way. Together, these documents contain a road map for the revitalization of a major regional commercial district.

The goals of the West Auburn Boulevard Special Planning Area Ordinance (SPA) and Streetscape Master Plan (SMP) are to

- Create a strong, recognizable community identity;
- Provide a safer and more effective environment for pedestrians, bicyclists, motorists, and business owners;
- Increase visibility to West Auburn Boulevard from passing traffic on the Capital City Freeway (also known as Business Loop 80);
- Promote infill and commercial development by supporting existing businesses with business improvement projects;
- Recommend land uses, policies and improvements that will encourage a variety of business ventures and strengthen the competitiveness of the Plan area;
- Strengthen urban form by providing an enhanced edge to the Arden-Arcade community; and
- Reinvest in retail and service components that service the regional and local communities.

This document sets clear goals for future development that will make West Auburn Boulevard more competitive to receive future funding for proposed improvements.

The initial step in developing the SPA and SMP was to conduct a site inventory and analysis. Site data from aerial photographs, field observations and measurements were collected, and site photos were compiled. Background research included traffic calming measures and previous studies of other street beautification projects in the County of Sacramento.



Current Conditions



Proposed Improvements

After a number of preliminary design concepts were developed and design review meetings were held, a variety of conceptual plans were presented to community leaders and residents at public workshops and charettes. Community members were invited to ask questions and provide feedback on the concepts. Their responses were then incorporated into a Draft Concept Plan.

The Draft Concept Plan combined feedback obtained through public workshops from the community, stakeholders and the Technical Advisory Committee. The Draft Concept Plan was presented at a second charette, where community members provided additional feedback that was incorporated into the SPAO and SMP detailed in this document.

Primary components of the Special Planning Area Ordinance:

- Form-based regulations;
- Upgrade of design quality;
- Optimize use of corner parcels and available sites as “neighborhood gateways”;
- Establish a theme identity;



Current Conditions



Proposed Improvements

- Create safe connections to transit stops;
- Upgrade the connection to Haggin Oaks Golf Course;
- Establish pedestrian connections to adjacent neighborhoods.

Primary components of the Streetscape Master Plan:

- “K-rail” separation between Caltrans and Boulevard rights-of-way;
- Strategically located raised and planted medians;
- Decorative paving;
- Public art;
- Consistent separated sidewalks and upgraded bus stop facilities;
- Bike lanes;
- Landmark and monument signage;
- Utility undergrounding;
- Emphasis on “neighborhood gateway” intersections;
- Consistent right-of-way tree and ground plane plantings;
- Regular maintenance operations.

SECTION ONE: Introduction to the Plan Area



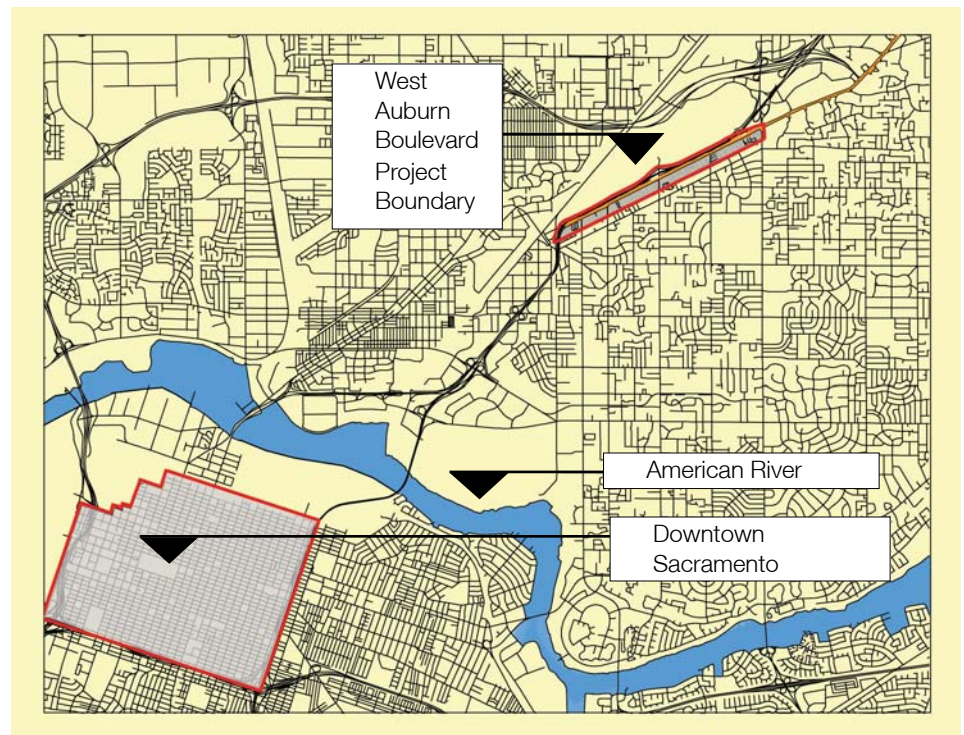
Auburn Boulevard today

Section One summarizes the history and existing conditions along West Auburn Boulevard, as well as the pattern of land use and development that has characterized the Plan area. The public input process undertaken to create the West Auburn Special Planning Area Ordinance and Boulevard Streetscape Master Plan is also detailed.

A. Plan Area

The West Auburn Plan area comprises 118 acres within both the City and County of Sacramento. The two-mile commercial corridor fronts Interstate Business Loop 80 or Capital City Freeway. The Boulevard extends through Sacramento County from the west edge of the Plan area to Placer County. The Plan area is immediately south of the Capital City Freeway, between the Marconi Curve on the west and Watt Avenue on the east. A Caltrans right-of-way abuts Auburn Boulevard on the north. The Special Planning Area Ordinance (SPA) and the Streetscape Master Plan (SPA) address the street and properties along the south side of the Boulevard. Due to its length and the potential for other projects elsewhere along Auburn Boulevard, this project is referred to in this document as the *West Auburn Boulevard Special Planning Area Ordinance and Streetscape Master Plan*.

The Plan area was designated as the Auburn Boulevard Redevelopment Area by the Sacramento Housing and Redevelopment Agency (SHRA) in October 1992. In response to extensive physical, economic and social deterioration, the Agency successfully partnered with other agencies to address health and safety issues affecting the Boulevard. Since the



West Auburn Boulevard Context Map



Redevelopment Area was established, crime and other vices have been substantially reduced in the area, infrastructure has been overhauled, and new investment has occurred along the Boulevard.

Freeway accessibility and visibility are key assets for West Auburn Boulevard properties. Four freeway ramps provide immediate access to the Boulevard. Lots that front the Boulevard tend to be narrow and deep. Although the depth allows for ancillary uses to the rear of the properties, their narrowness impedes high impact development along the street front, which would benefit from increased visibility. Parcel assemblage is encouraged to provide additional width and greater flexibility for re-use and design.

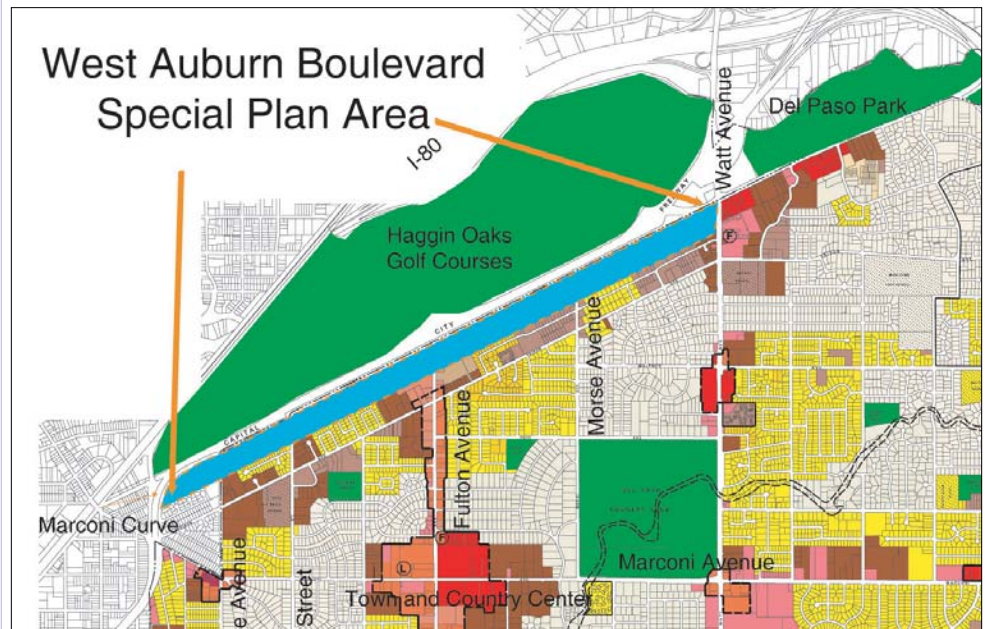
Further challenges for redevelopment include inadequate public improvements such as unsightly freeway barriers, missing or deteriorating sidewalks, insufficient landscaping, and poorly maintained housing. Notwithstanding these challenges, general and light commercial uses such as auto dealerships, furniture stores, auto service centers, and hotels have been successfully attracted to the Boulevard. These land uses contribute to the Plan area's positive image and bright prospects for future redevelopment.

The Plan area currently is experiencing a period of significant transition. The older, highway service-oriented businesses, such as older lodging facilities, mobile home parks, and a scattering of highway-oriented restaurants will be replaced in time by emerging retail and service businesses that relate to the larger community. These uses will benefit from freeway visibility, but will no longer depend solely on the patronage of long distance travelers. In time, the Boulevard will become a retail and service center serving the regional and local communities.

B. Background Reports and Resources

To prepare this Report, the following documents were consulted:

- 2010 Sacramento City/County Bikeway Master Plan (1993)



West Auburn Boulevard Special Plan Area

- Sacramento County Pedestrian Design Guidelines – Draft (2005)
- Sacramento Housing and Redevelopment Agency Auburn Boulevard Community Redevelopment Area Implementation Plan (2005-2009)
- Sacramento County Best Practices for Complete Streets (2005)
- Sacramento County Best Practices for Bicycle Master Planning and Design (2005)
- Auburn Boulevard Redevelopment Area Community Plan Amendment, Rezone, and Use Permit FEIR (1996).

C. Site History and Background

The West Auburn Boulevard Corridor forms the north edge of the Arden-Arcade Community, which encompasses about 13,000 acres or 21 square miles. The area was part of the Rancho del Paso land grant, which was obtained by James Ben Ali Haggin circa 1860. Haggin bred thoroughbred racehorses on the ranch before selling the property in 1910 to the Sacramento Valley Colonization Company, which intended to



Business 80/Capital City Freeway - Opening Day

subdivide the land. Much of Arden-Arcade began to develop in the 1940's coincident with the construction of Town and Country Village (the first suburban shopping center in Northern California). Most of the population growth in the area occurred between the 1950's and 1970's, when an estimated 75% to 80% of the community was developed with suburban uses. Arden-Arcade is now one of the most intensely developed areas of Sacramento County.

Another notable characteristic of the Plan area is the former role of the Boulevard as a link in the first trans-continental highway. Highway 40 also called the Lincoln Highway, stretched from San Francisco to Chicago and provided a key link in the nation's transportation network. Although replaced by the current State Route 51 (the Capital City Freeway) on the north side of the Boulevard, the Plan area retains many elements of that earlier era, most notably in the older motels and traveler's rests scattered along the Boulevard. Elements of this historical highway provide a potential theme for the design of features and landscaping along the Boulevard.

An interesting characteristic of the community is that the major streets that cross Auburn Boulevard are named in honor of 19th and early 20th century inventors: Howe, Bell, Fulton, Marconi, Watt, Morse and other notables. These names provide theme and interpretive opportunities for the Plan area and the Arden-Arcade community as a whole.



Historic Auburn Boulevard





Public Workshops



In 1999, the County of Sacramento undertook an improvement project along Fulton Avenue in conjunction with the Fulton Avenue Improvement Association. The first three phases of this project have been completed; the fourth and final phase is nearing construction. The impact of this project is significant to West Auburn Boulevard, since Fulton Avenue bisects the Plan area and provides the only link within the Plan area to areas north of the freeway.

D. Role in the Sacramento County General Plan Update

The West Auburn Boulevard corridor is one of several throughout the county identified as centers for significant new growth in the General Plan update. Such older commercial corridors are envisioned as sites for intensified, mixed-use developments that provide a cumulative significant area to absorb new population in coming decades.

Although relatively small in area compared to many of the other corridors, West Auburn Boulevard has substantial opportunities for higher density in-fill development and access to existing public transit.

E. History of Reinvestment

Auburn Boulevard is clearly in an economic transition as older travel-oriented commercial businesses are replaced with new investments in buildings and improvements. Mostly recently new investment has been primarily in lodging and new auto sales facilities. Many older buildings are used for a variety of small office, service and retail uses. Others are used for construction yards, auto storage yards, storage buildings and small shops. These buildings and the surrounding improvements (i.e. landscaping, sidewalks and parking areas) are often in poor condition. Notable conditions of deteriorated or poor-quality public improvements along the Boulevard include:

- Broken or missing sidewalks within the right-of-way;
- Deteriorating and misdirected drainage improvements;
- Irregular street lighting;
- Broken and sagging fencing along the highway;
- Lack of landscaping along the highway;
- Lack of accessibility to bus stops;
- Lack of enforcement with respect to current codes;
- Lack of general maintenance in both public and private areas;
- Perceived lack of available off-street parking;
- Awkward lot sizes and configurations;
- Varying levels of visibility to and from the freeway;
- Outdated freeway access ramps;
- Excessive traffic speeds along the Boulevard;
- Oversized traffic lane widths and number of lanes; and
- Distracting overhead power lines.

F. Public Process

Throughout the course of drafting the Special Planning Area Ordinance (SPA) and Streetscape Master Plan (SMP), the County and SHRA felt it was important to forge a partnership among private property owners, Caltrans, SHRA, and various departments within the County and the

City of Sacramento. This outreach and collaboration occurred in several forms and addressed the community's and business owners' concerns and goals, and the technical issues that affect the future development of the Boulevard.

A stakeholder group was formed to represent both current and future property owners along the Boulevard. The stakeholder group represented more than 10 properties and the Auburn Boulevard Project Area Committee (PAC), an ad hoc committee of SHRA. The stakeholder group met four times through the planning process, and additional meetings with individual stakeholders were held to inform them of the Plan's progress.

A Technical Advisory Committee was formed to advise the Consultant Team on various technical issues existing in the Plan area. This committee included representatives from Caltrans, as well as representatives from Sacramento County's Departments of Transportation, Municipal Services Agency, Planning, Regional Sanitation, Environmental Review and Assessment, Sheriff's Office, and Community Services. The Technical Advisory Committee met a total of six times during the planning process.

Engaging the general public in the planning process is an important aspect in creating a Plan that enjoys public acceptance and support. A comprehensive outreach program was implemented to invite public participation in two design charrettes, which were held in February and March 2006. The purpose of these charrettes was to solicit ideas from the public for inclusion in the Plan, as well as to document progress as the Streetscape Master Plan developed.

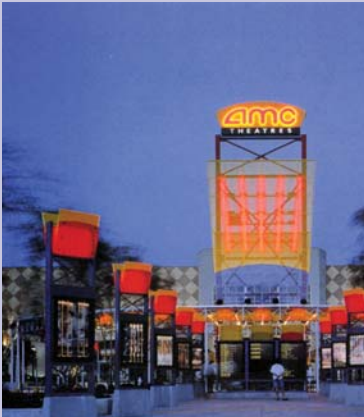
To generate public participation in the planning process, postcards inviting people to attend the design charrettes were mailed to more than 1,500 addresses within a 500-foot radius of the Plan area. Advertisements announcing the design charrettes were placed in the Sacramento Bee, Inside Arden and Arden-Carmichael News periodicals. Two newsletters summarizing the public input and presenting draft and final design concepts prepared by the Consultant Team were sent to the same mailing list.



SECTION TWO: Project Vision



Multi-use, multi-story structures will increase West Auburn Boulevard's visibility from the Capital City Freeway



High visibility signage attracts potential customers from the freeway

Section Two summarizes the vision, goals and objectives for the Special Planning Area Ordinance and Streetscape Master Plan that were established by the stakeholders and public through two community charettes.

A. Vision

Given its proximity to downtown Sacramento, the success of recent redevelopment initiatives, and the constrained availability of affordable sites for commercial development adjacent to a major freeway, West Auburn Boulevard is poised to become a signature business address and vital commercial destination within the region. Building on the intent of the Auburn Boulevard Community Redevelopment Area Implementation Plan, the West Auburn Boulevard Streetscape Master Plan and Special Planning Area Ordinance create a framework for redevelopment by identifying potential improvements that will attract business investment to the Plan area.

Key to the success of the Plan is the treatment of intersecting corners. The “crown jewel” of the Boulevard is the intersection of Auburn Boulevard and Fulton Avenue as this is the only location that allows full access from the freeway and into the geographical center of the Plan area. The redevelopment of the parcels on both sides of this intersection is vital to the success of this project and should be scrutinized accordingly to ensure that the vision and goals of this project are recognized through site design. The remaining intersections also play a vital role in the economic vitality of the Boulevard. These intersections are envisioned as mixed-use opportunities, where housing or office space is situated immediately above retail or service uses. These corners should be treated with significant site design care by creating buildings closer to the street and pedestrian spaces and placing parking behind the buildings.

In addition, the following vision statements apply to the Plan area:

- New buildings and streetscapes including trees, common signage, lighting and landscaping will enhance the overall image of the Boulevard and create an attractive portal to the Arden-Arcade community.
- The West Auburn Boulevard Plan area will develop into a center for auto sales and services, home and garden design, retail, commercial, recreation, leisure activities, and attractive housing opportunities.
- Restaurants, entertainment and recreation centers will serve the existing community residents and new residents along the Boulevard and will attract patrons from throughout the region.
- The Boulevard will expand and grow to become an economic engine for public services and continued economic growth that will extend to the surrounding neighborhoods.
- The Boulevard will provide commercial services and retail that are lacking in the adjacent community.



- The Boulevard will become linked with public amenities such as the Haggin Oaks Golf Course.
- The Boulevard will become a notable landmark area known throughout the region for the high quality streetscape and building architecture. The design quality will be reflected in the Boulevard through:
 - o Architecture
 - o Signage
 - o Streetscape
 - o Lighting
- The landscaping, signage and signature elements located along the public streets and the architecture of individual buildings will provide a form of public art.
- Commercial uses will not be oriented only to street fronts. Retail and leisure recreation uses (i.e. restaurants) penetrate to the rear of individual parcels and “open up the block.”
- The combination of attractive views toward the golf course, proximity to downtown jobs and new neighborhood services will create opportunities for high density housing.
- Creation of more intensive land use, new public services, new shopping destinations and new higher density housing will increase use of public transit.
- The history of the Arden-Arcade Community and surrounding area will be celebrated in the signage and streetscape themes.

B. Goals

The redevelopment strategy for West Auburn Boulevard focuses on leveraging the key attributes of the Plan area—location, convenience, and visibility—to provide an attractive framework for future growth. To achieve the goals inherent in the vision for West Auburn Boulevard, this Master Plan, recommends achievable ways to

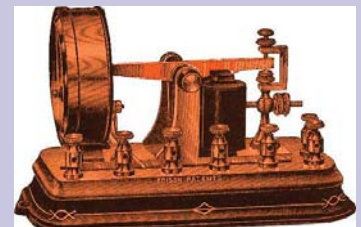
- Develop a Land Use Plan and Special Planning Area Ordinance that address, encourage and support quality infill commercial development along the corridor;
- Create a strong, positive community identity through the use of high quality signage, enhanced landscaping, common interest marketing, and media awareness of West Auburn Boulevard’s heritage and environment;
- Improve public rights-of-way to enhance the aesthetic appeal of West Auburn Boulevard to businesses and shoppers;
- Make full use of the existing public investment in infrastructure including public transit;
- Eliminate infrastructure deficiencies along with completing the streetscape improvements, to further strengthen the long-term competitiveness of the Plan area;
- Create a dynamic business environment that attracts new investment and helps to realize West Auburn Boulevard’s potential as a commercial destination within the region;
- Maximize new development opportunities, but minimize new demands on natural resources, notably clean air, clean water and energy;
- Develop improved freeway access, and heighten visibility of the Boulevard for people traveling on the freeway;
- Promote quality design through improvement projects that serve as catalysts for attracting additional investment;

- Fulfill the unique retail and marketing opportunities afforded by the traffic and regional location;
- Enhance the adjacent neighborhood and Arden/Arcade community;
- Encourage new auto sales within appropriately sized parcels centered on Fulton Avenue;
- Establish a landmark identity and style to create a district recognized throughout the region;
- Promote infill and commercial development; and
- Support established businesses and provide an employment base for the community.

C. Objectives

The core objectives relate to providing physical improvements and establishing a policy framework and standards for implementing the economic development goals of the Plan. It is inherent in the relationship of economic development and physical improvements that they are mutually beneficial. Economic development provides the funding resources to create additional attractive site improvements and buildings. In turn, these improvements attract additional investments.

- Analyze proposed land uses for West Auburn Boulevard’s commercial corridor;
- Integrate input received at project management team meetings, stakeholder workshops and public charettes;
- Utilize vacant land to promote appropriate in-fill commercial development that supports well-established businesses and provides an employment base for the community;
- Create a strong identity and develop a vibrant, contemporary commercial corridor that includes:
 1. Themes to enhance identity;
 2. Landscaped medians and colored pavers;
 3. Improved streets, sidewalks, curbs and gutters for better vehicle, pedestrian and bicycle traffic;
 4. Lighting, including decorative lighting and traffic signals;
 5. ADA compliance;
 6. Enhanced freeway frontage, visibility and aesthetic enhancements;
 7. Intersection and off-ramp enhancements;
 8. Parking and reciprocal access opportunities and improvements;
 9. Gateway signage and other identifying enhancements and themes for the commercial corridor;
 10. Specialty treatments at bus stops, walkways to businesses, driveways, and other points of interest;
 11. Frontage trees, landscape and hardscape improvements;
 12. Future traffic congestion issues;
 13. Establish Design Guidelines that address architecture, landscape, lighting, set-backs, signage and other property amenities and standards;
 14. Identify non-conforming uses to be phased out and/or prevented as future uses that negatively affect commercial viability;
 15. Identify current policies (including setback requirements, land uses, etc.) to be re-evaluated and possibly altered;





16. Establish distinctive gateways to the Arden-Arcade community;
17. Establish a strong and attractive sense of place and identity for the Boulevard;
18. Enhance traffic safety along the Boulevard and the adjacent streets.

SECTION THREE: Special Planning Area Ordinance

Form-based regulations are prescriptive. That is, they guide development to desired outcomes by example rather than by precluding or prohibiting certain development characteristics.

Section Three details how the Special Planning Area Ordinance creates a single new land use zone classification that accommodates all land uses envisioned in this Plan area.

The West Auburn Boulevard Special Planning Area Ordinance (SPAO) will include land uses that are not typically combined in conventional land use zones. The mix of land uses will be regulated with “form-based” zoning that allows different but appropriate land uses based on the site conditions and the relationships between land uses rather than rigid land use categories.

A. The Special Planning Area Zone Ordinance

The authority for Special Planning Areas (SPAs) is established in the Sacramento County Zoning Ordinance. The purpose of the SPA is to regulate properties that have unique environmental, historic, architectural, or other features which require special conditions not provided through the application of standard zone regulations.

As established in the Zoning Ordinance, the Special Planning Area Ordinance shall provide for the following matters:

- A list of permitted uses;
- Performance and development requirements relating to lot area, intensity of development on each lot, parking, landscaping and signs;
- Other design standards appropriate for the specific site and development; and
- Reasons for establishment of an SPAO Land Use Zone on the particular property.

Relationship to the County Zoning Ordinance

The SPAO is intended to be comprehensive and to replace the current zoning and land use regulations within this Plan area. However, the SPAO does not address certain detailed standards that are best addressed on a County-wide basis, for example, parking requirements. Where the SPAO is silent, the Sacramento County Zoning Ordinance shall apply.

Form-Based Regulations

Form-based codes differ from conventional codes in terms of the substance of the standards they contain, the mechanisms by which they are implemented, and the built form they produce. The land use and design regulations are derived from the urban form rather than simple categorization of the land uses according to the similarity of use. The regulations are affected by such factors as the location on the street, the relation to adjacent land uses, the scale of the buildings, and the type of uses. Although there is only a single zoning standard for the entire

Form-based codes are typically more prescriptive than conventional codes in establishing design requirements for development. That is, they provide more detailed regulations based on a specific vision of the intended built result, and regulate certain features of development beyond those covered by conventional codes.

Plan area, the development regulations define differences from location to location depending on use and other factors that guide land use.

Form-based codes are typically more prescriptive than conventional codes in establishing design requirements for development. That is, they provide more detailed regulations based on a specific vision of the intended built result, and regulate certain features of development beyond those covered by conventional codes. (Adapted from “Removing Obstacles to Blueprint Implementation: Scoping Services for Model Form-Based Code or Alternative Strategy, “Sacramento Area Council of Governments, Administrative Draft Report, 5 April 2006).

The purpose of form-based regulations (also referred to as “form-based code”) is to allow design creativity within a flexible context and encourage maximum use of the development area. However, the purpose of this SPAO is also to provide certainty and timeliness in the process for review and approval of development applications. Therein lies a challenge for the form based regulation: to provide design flexibility and a certain amount of creativity in the planning process, yet to also limit the discretionary actions to a level that can be addressed in an administrative process.

The approach taken in this SPAO is to first define the objective of the standards and then to define standards that require a level of performance or a condition that can be readily determined at an administrative level. It is expected that there will be many ways to achieve some standards (such as parking area shading requirements) and thereby allow considerable design flexibility. Other standards are inherently more specific (such as side Yard setbacks) that are absolute minimum requirements.

Application Form and Process

New development and remodels of existing structures that exceed 10,000 square feet in floor area shall make an application for Design Review pursuant to the standards and guidelines set forth in this Plan. The form of application shall be as determined by the County Planning Director and shall include information requirements consistent with the submittal requirements established in the Community Design Guidelines: Commercial and Mixed-Use Development. (See Sacramento County web site for Development Application Forms.)

The non-discretionary development applications that comply with the standards established in this SPAO shall be approved in an administrative review process as outlined in the Appendix of this Plan.

A development application that deviates from this SPAO such that it must be referred to the Arden-Arcade Community Planning Council and/or Board of Supervisors may require only a Mitigated Negative Declaration or supplemental environmental documentation to comply with the California Environmental Quality Act (CEQA).

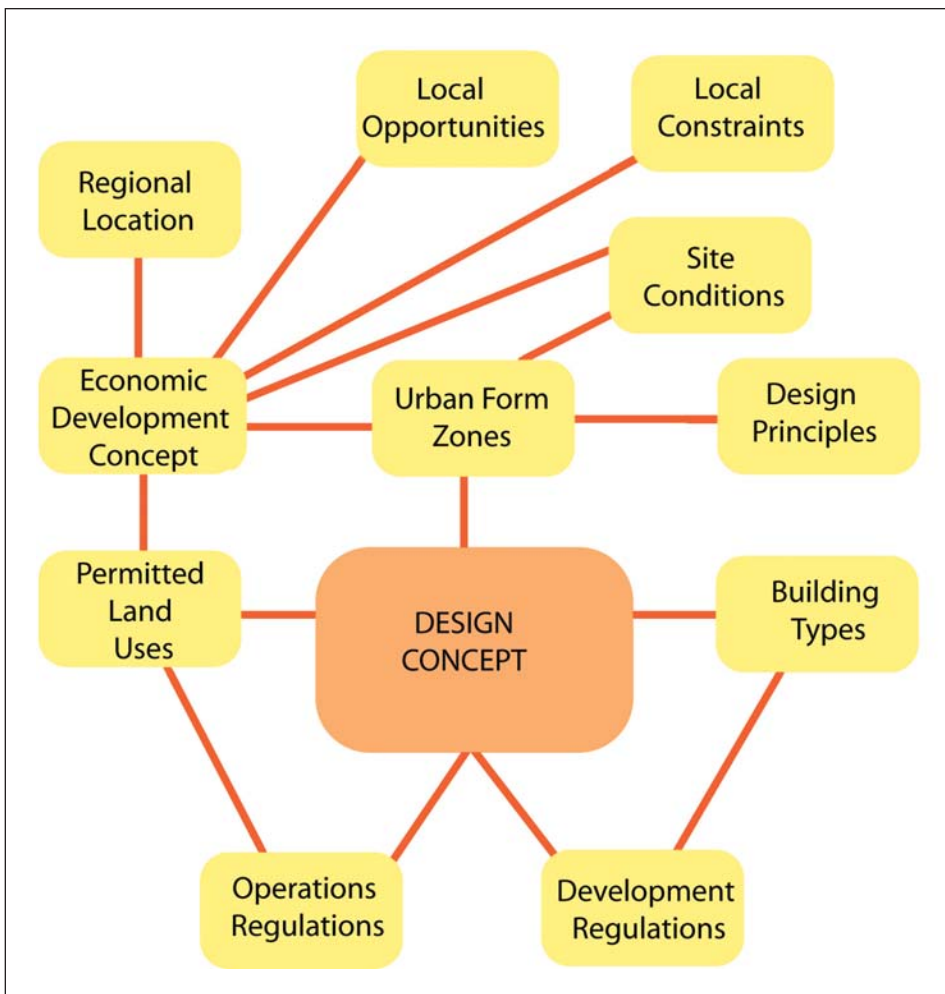
Organization of the Special Planning Area Ordinance

The SPAO describes the existing planning context including opportunities and constraints that suggest the vision for future development. The Plan defines a concept of the economic development opportunities in the Plan area in terms of an appropriate mix of land uses and design principles and strategies. This concept defines a list of appropriate land uses that will be

permitted. This SPAO provides operational characteristics regulations to allow certain permitted uses that would otherwise require a conditional use permit.

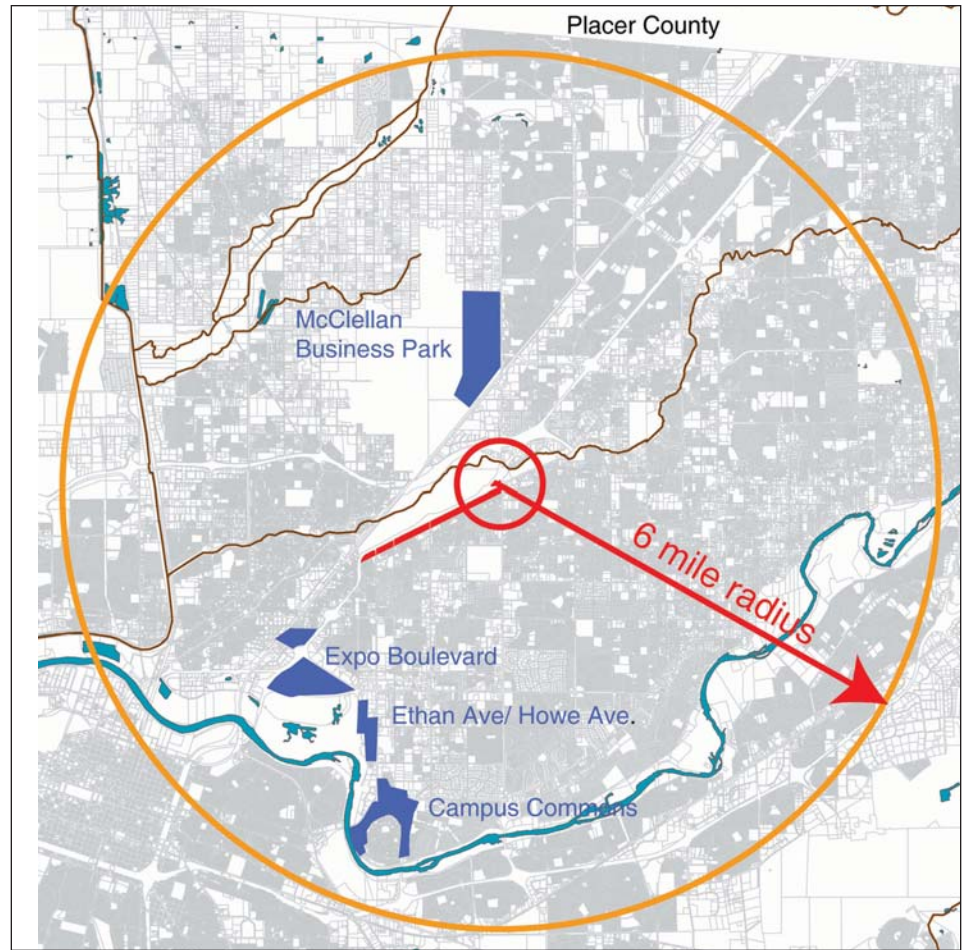
In addition to the list of permitted uses and the development standards found in a conventional zoning ordinance this SPAO also defines urban form zones and building types. The development standards are tailored to the specific conditions within and vision for this Plan Area. All land uses are permitted at any location within the Plan Area, however, compliance with the form based zones will direct certain uses and building types toward more appropriate locations and away from less appropriate locations.

The building type descriptions and the urban form zones defined in the following sections provide a guide to the intended uses and development patterns. This approach will avoid incompatible uses but allows for creative design solutions that may allow a use or combination of uses to be located where it would otherwise be considered inappropriate.



Schematic of the Special Planning Area Ordinance Organization

Within this radius is an established population of more than 400,000.



Local Context Map

B. Community Planning Context

The opportunities for new economic growth and development on West Auburn Boulevard reflect the character of the regional market area and the specific character, history and land uses of the surrounding community.

Location in the Region

West Auburn Boulevard is situated near the geographical center of the established population in the northeast quadrant of Sacramento County. Businesses can attract patrons from the immediate neighborhood to the south along major road connections that extend south into the Arden-Arcade Community (Howe Avenue, Fulton Avenue and Watt Avenue). The Plan area is also easily accessible from neighborhoods farther to the east along Auburn Boulevard.

The Plan area is within one mile of the Capital City Freeway and the I-80 interchange known as the “Madison Split.” The entire northeast Sacramento region is within a reasonable driving distance of the Plan area. A six mile radius centered on the east end of the Plan area at Watt Avenue would extend from Roseville on the north to Folsom Boulevard on the south, and from Sunrise Boulevard on the east to downtown Sacramento and the South Natomas area on the west. Within this radius is an established population of more than 400,000.

Proximity to Employment Centers

Nearby employment can provide significant business support for several of the business activity types envisioned for the Plan area. Employees would patronize restaurants, entertainment, leisure activities, specialty retail shops and personal services during their day and after work.

Currently, the primary employment areas are in the southwest quadrant of the community near Ethan Way and Howe Avenue. Although Watt Avenue to the north of the Capital City Freeway provides commercial land use, there are currently few food services. McClellan Business Park, located approximately two miles north on Watt Avenue, has the potential to provide a patronage base as employment at the business park grows.

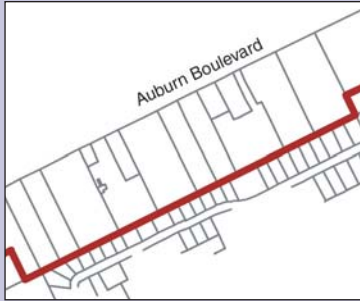
Increased employment along the Boulevard and within a reasonable drive time, and expanded restaurant service, could make the Boulevard a more attractive daytime destination.

Arden-Arcade Community



Arden - Arcade Community Area

Today, Arden-Arcade is one of the most heavily developed areas of urban Sacramento.



Existing Parcels

The shape of the parcels is important because it determines to a significant degree the types of businesses that will be viable along the Boulevard as it becomes more developed and land values rise.

The West Auburn Boulevard SPAO is the north edge of the Arden-Arcade community. The community encompasses about 21-square miles and extends south to the American River. The Arden-Arcade community provides a rich history commencing with the early settlement of the Sacramento region.

The community is part of the Del Paso Ranch, nationally renowned as a significant center of racehorse breeding in the late 19th century. The naturally appealing setting of the community along the south side of Arcade Creek lead to early development as a ranching area.

Much of Arden-Arcade began to develop in the 1940's with the Town and Country Village shopping center (the first suburban shopping center in Northern California). Most of the population growth in Arden-Arcade occurred up through the 1960's and 1970's. An estimated 75 percent to 80 percent of the community was developed with urban uses.

Today, Arden-Arcade is one of the most heavily developed areas of urban Sacramento. Despite the intensity of uses, a combination of contemporary neighborhoods, graceful older residential areas, substantial frontage along the American River Parkway, and unique shopping areas have created a distinctive charming character for Arden-Arcade.

C. Planning Design Context

Opportunities for development are strongly influenced by the physical conditions along and around the Boulevard. The following describes the key features of the Plan area that affect future development.

Available Land Area

The Plan area encompasses 71 individual parcels totaling 123.3 net usable acres. Approximately 45 percent of the net usable area, or 55 acres, is currently vacant or under-utilized (refer to the discussion regarding "transitional" land uses).

Shape and Size of Individual Parcels

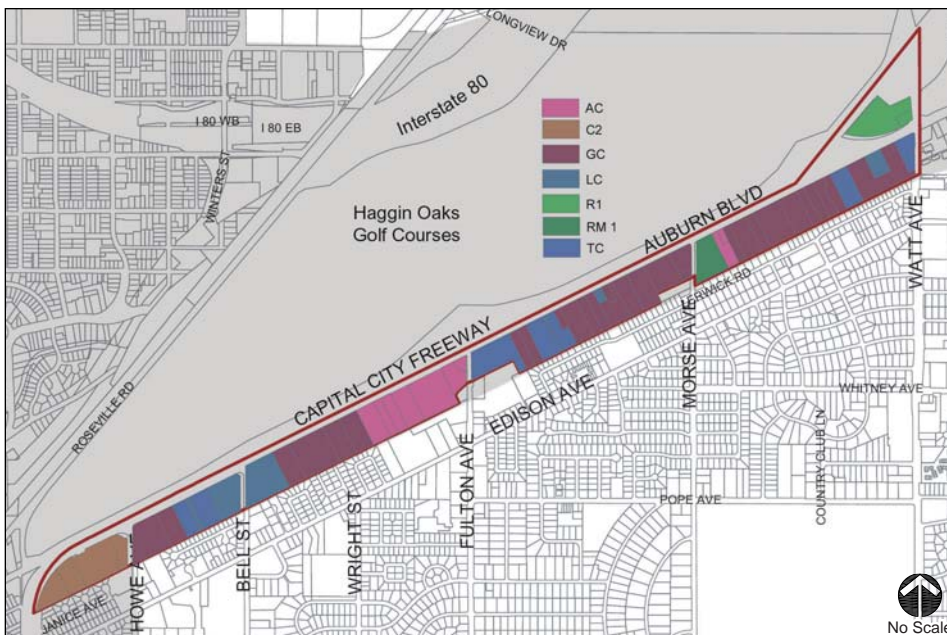
Properties fronting the Boulevard are typically of uniform depth of 450' from the edge of the right-of-way. The parcel depth is comparable to the length of a single city block in downtown Sacramento. This lot depth (front sidewalk to rear lot line) is too shallow for many contemporary commercial uses, such as conventional supermarket-based centers and "big box" retail stores (such as Home Depot or Wal-Mart), but is appropriate for furniture stores and other uses of similar size envisioned in this SPAO. The Deseret Industries store and complex are examples of larger retail stores.

Conversely, the lot depth is too great for many narrow parcels with building uses that rely on Boulevard frontage. Smaller uses do not need the entire site. Consequently, the rear portions of parcels are currently under-utilized in some instances.

The lot width varies from approximately 50' to 485'. There is no standard lot width; however, lot widths of approximately 110' occur more frequently than any other. A lot that is 110' by 450' has a depth-to-width ratio of more than 4:1.

Table
Existing Zone Classification

	Land Use	Parcels	Acres
(AC)	Auto Commercial	10	15.2
(C2)	Commercial (City of Sacramento)	4	9.6
(GC)	General Commercial	32	56.3
(LC)	Limited Commercial	6	10.4
(R1)	Single Family Residential	2	5.3
(RM-1)	Mobile Home Subdivision	4	10.0
(TC)	Highway Travel Commercial	13	16.5
	Total	71	123.3



Existing Land Uses

Existing Land Use

Many existing commercial uses reflect the historic role of the Boulevard as a major highway. The area consists of an eclectic mix of hotels, auto dealerships, home retail stores (i.e. furniture, lighting, and imports), a variety of other destination commercial retail, music and dance destinations, small restaurants, residential uses (i.e. senior and mobile home parks), and a substantial number of vacant parcels.

New auto dealerships are an active category of new business development and suggest the continued expansion of these businesses as a significant use.

Small pockets of residential uses are found in older mobile home parks that front onto the Boulevard.

Existing Zoning Classifications



Existing zoning and land use adjacent to the Plan Area includes a high percentage of multi-family residences.

Existing zoning in the Plan area includes seven distinct zone districts, including zone (C2) in the City of Sacramento. The zone districts are primarily commercial classifications, but also include two residential zone classifications. The existing zones and the acres allocated to each are summarized in the Existing Zone Classification Table on page 3.5.

The adoption of the West Auburn Special Planning Ordinance would rezone all existing County zone classifications into a single form-based land use zone as described in this SPAO.

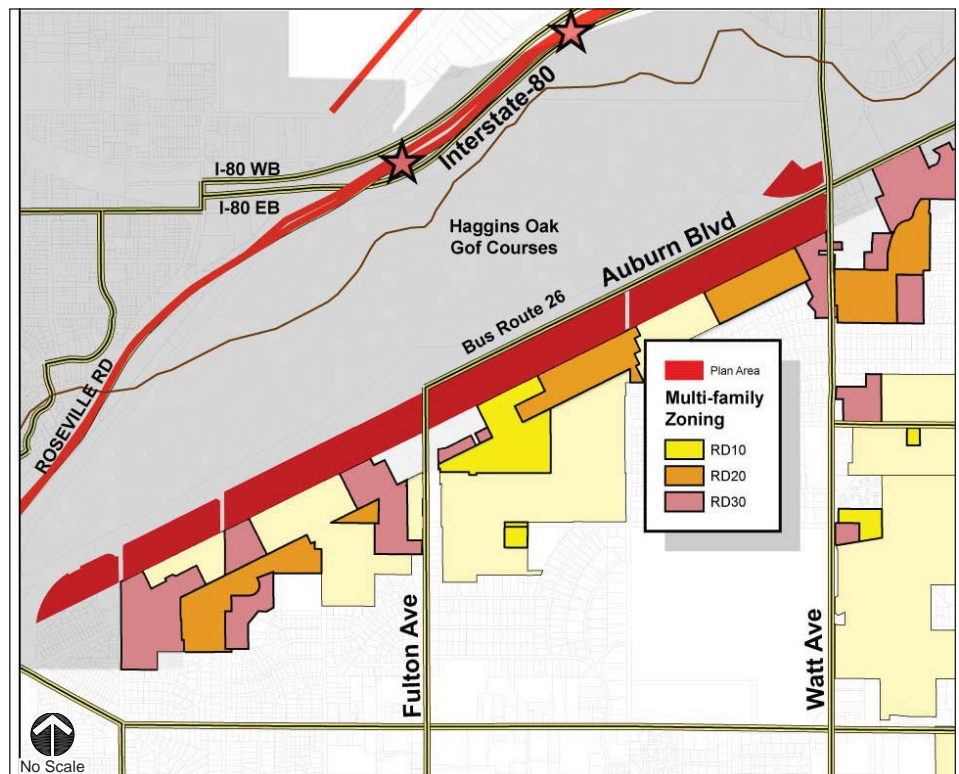
Adjacent Land Use

To the south of the Boulevard, commercial uses abut older residential neighborhoods that include suburban style single-family homes and apartments.

Approximately 187 acres adjacent to the Plan area is designated and developed as multi-family residential uses (R10, R20 and R30). If built at the maximum density allowed under the current zoning, this would yield approximately 4,255 dwelling units. The existing apartments appear to be developed at more modest densities of approximately 15 to 20 dwelling units per acre. Nonetheless, this area is substantially more dense than most suburban communities in Sacramento County.

This is an unusual opportunity to supplement the existing residential core with additional residential uses along the Boulevard. The high density residential use and supporting commercial uses along an existing transit corridor would be the foundation for a significant transit-oriented development core.

Access to Public Transit



Adjacent Land Use

Regional Transit Bus Route 26 runs through the Plan area from Watt Avenue to Fulton Avenue. The route connects to the Watt Avenue Light Rail Station and south along Fulton Avenue to connect to the University/65th Street Light Rail Station. Bus stops are located along the south side of the Boulevard at a number of locations, but west bound buses stop on the north side of the Boulevard at only one location, opposite the Deseret Industries Thrift Store.

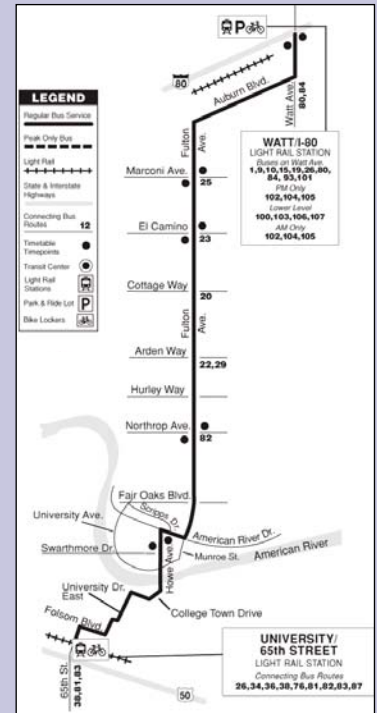
The Watt Avenue Light Rail Station is located approximately one-half mile north of the Boulevard.

Local Street Connectivity to the Adjacent Residential Area

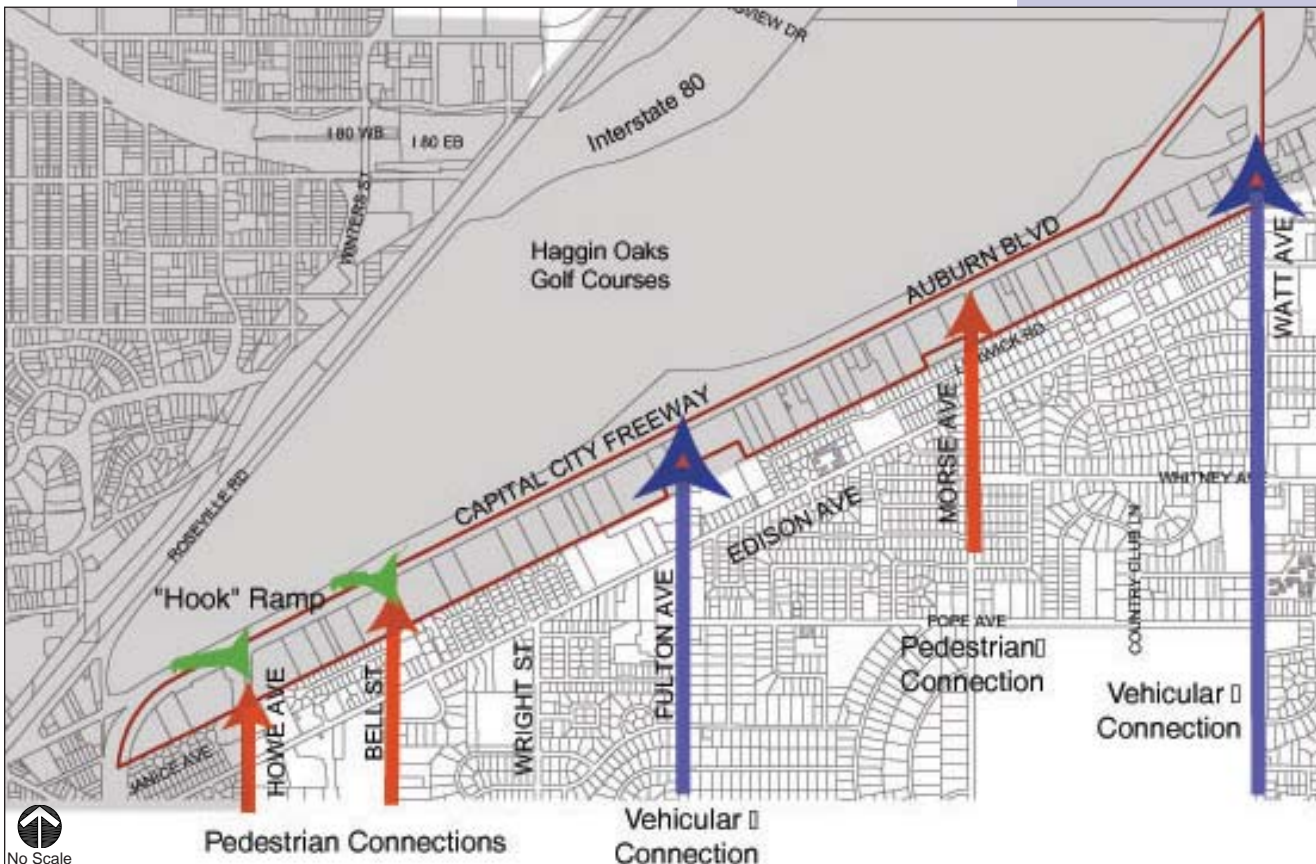
Unlike many other urban and suburban boulevards, Auburn Boulevard has relatively few intersections with local streets that extend into the adjacent neighborhood.

Intersections are limited to the major north-south oriented streets that are spaced at approximately one-half mile intervals. Watt Avenue and Fulton Avenue are major commercial streets with little or no residential uses near the Boulevard. Howe Avenue, Morse Avenue and Bell Street have residential neighborhoods adjacent to the commercial uses, but these neighborhoods have a very weak relationship to the Boulevard due to the lack of neighborhood-serving businesses.

The adjacent neighborhoods appear to have only a limited relationship to the Boulevard in terms of pedestrian activity. This results from the lack of street connectivity, relatively long block length between major streets and the lack of local services on the Boulevard.



Bus Route #26



Connectivity

Freeway Access

Access from the Capital City Freeway to the Boulevard occurs at Howe Avenue, Bell Street, Fulton Avenue and Watt Avenue. Watt Avenue and Fulton Avenue are full interchanges that offer access and egress in all four directions. However, these are older facilities that are constrained by existing development.

Bell Street and Howe Avenue are short “hook” ramp interchanges that allow only eastbound egress and ingress to the freeway. The hook ramps allow traffic exiting the freeway to cross the path of traffic accelerating onto the freeway.

Adjacent Amenities (Haggin Oaks Golf Courses)

The Haggin Oaks golf courses to the north of the freeway provide a visual amenity for the Plan area. The courses and ancillary facilities are an attractive green space that contrasts with the urban intensity of the freeway and Boulevard. Although separated from the Boulevard by the freeway and often obscured from view at ground level, the golf courses can be a significant visual asset when viewed from the second or higher floor of buildings on the Boulevard.



Haggin Oaks Golf Courses

View from the Freeway

Although adjacent to a major freeway, not all properties fronting on Auburn Boulevard have high visibility. For motorists in the west-bound lanes of the freeway, the center median barrier effectively blocks the view of the first level of all buildings. Only the second floor of buildings, a few communications towers and scattered tree tops, most notably clusters of tall palm trees, are visible from the west-bound lanes. In addition, much of the view is obscured by a high-tension power line on metal poles located along the freeway edge.



The motorist’s view from the east-bound lanes, in some cases, is directly to the building fronts on the Boulevard. However, the east-bound view is also obscured at locations by grade differences (notably near Fulton Avenue), vegetation and the fence located along the edge of the right-of-way.



Freeway Views

The freeway sweeps away to the north as it approaches Watt Avenue from the west and the Boulevard frontage is not visible from the freeway. The view at this location is a rental storage facility located between the freeway and the Boulevard.

A direct view of the ground level of buildings occurs at approximately 50 percent of the Boulevard frontage through the Plan area.

Noise

Freeway noise will be a factor in considering residential development fronting on the Boulevard. Typical sound walls along the freeway will not be practical where it is important to maintain a view toward Boulevard businesses. Buildings that house noise-sensitive uses will need to incorporate internal noise mitigation measures or be located near the Fulton Avenue bridge where the freeway is depressed below grade.

Air Quality

Traffic generated air pollution will also be a factor in considering residential development, some employment intensive uses and other sensitive uses along the freeway.

High Tension Power Lines

The view along the Boulevard and the view from any future buildings to the golf courses will be affected by power lines along the sides of the Boulevard. On the south side, 12 kv electric power lines as well as telephone lines can be placed underground in the future streetscape improvements.

The larger high-tension electric lines on steel poles located on the north side of the street cannot be located underground and will remain as a major visual blight. Trees cannot be placed under these power lines.

D. Plan Area Vision: Land Uses and Building Activity Types

The vision for the Plan Area includes two distinct components: retail sales and specialty services, leisure and recreation services that have a regional draw; and retail and businesses that serve the residents and employees in the local market area. Residential uses will be mixed in with other uses or will be stand alone uses. The uses envisioned in the Plan Area are listed in the permitted uses table and are summarized in the following description of different building types.

A variety of land uses can occur at any location on the Boulevard in a variety of building types. The development regulations described in Section Four are intended to accommodate the following building types:

- Office
- Auto Sales and Related Uses
- Specialty Retail
- Furniture and Home Improvement Retail
- Leisure, Entertainment and Recreation Uses
- Neighborhood Retail and Services
- Cluster Development
- Small Front Retail
- Multi-Family and Residential/Mixed Use

The following sections describe the design intent and the appropriate land uses for each building type.

Office

The West Auburn Boulevard corridor has yet to establish a presence in the regional office market. Existing office uses include only the medical office at Bell Street and a scattering of older single story buildings. However, a number of factors suggest that the Boulevard can develop a secondary office market. Among these are the proximity to downtown; the proximity to Cal Expo and the lodging facilities and secondary office market in the Arden Way area; the golf courses adjacent to the Plan area; freeway access; and high visibility from the freeway.



The shape and size of the larger parcels could accommodate multi-story office buildings in an attractive landscaped setting with surface and/or structured parking.

Office uses would occur in either stand alone buildings or mixed with high density residential, restaurants, other services and incidental retail uses. The Boulevard provides an opportunity for tall buildings to offer attractive views to the golf courses and Arcade Creek to the north.

Lodging

Lodging will continue to be a viable commercial activity along the Boulevard. New lodging facilities along the Boulevard are modern versions of the traveler and business motels that have been a mainstay of the old Boulevard. The Capital City Freeway is not the primary travel route for long distance travelers, but the location continues to be convenient for travelers and business people in the northeast quadrant of the region.

Auto Sales and Related Uses

On-going investments in new auto sales facilities suggest that auto sales, service, and related businesses could be a key component of the Boulevard's economic development. The properties provide high visibility along the freeway, and the width and depth of many parcels are well suited to auto sales operations. The SPAO envisions the Boulevard continuing to grow as a regional auto sales center.

Auto-related businesses would include new auto sales, auto parts sales, auto repair services and auto rental services.

In addition, the auto sales and service businesses would create a need for customer service and convenience businesses. Examples include: a hair salon or barber and a coffee shop/cafe where customers could enjoy a meal or a cup of coffee while waiting for service on their vehicle. These businesses would also serve the local community and freeway travelers.

New auto retail is a specialized activity that requires a specialized building type. Unlike most large retail uses, the interior sales floor is an extension of the exterior sales area (the parking lot). The exterior parking is substantially greater in proportion to the interior sales area and thus the building covers a relatively small portion of the site compared to other retail buildings. The auto sales building type also differs in that it often includes ancillary service and sales activities that may be located within the primary building or in separate buildings on the property. Street exposure is a dominant concern for auto sales.

Extraordinary signage, lighting, flags, banners and similar devices to attract attention to the sales area are typical. Building design is likewise used as a means to attract attention and to serve as a signature for the particular brand.

The challenge is to provide a highly visible sales area while maintaining a sense of quality and compatibility with the overall Boulevard design.

Large Box Retail



The Large Box building type includes any building with a cumulative facade of 150' or greater oriented to the street. These buildings may include a single large use or have multi-tenants.

Furniture and Home Improvement Retail Activity

Furniture and home improvement retail stores are a typical form of large box buildings, but also occur in smaller buildings along the Boulevard.

West Auburn Boulevard has long been the location of several furniture and home decorating businesses that benefit from the freeway visibility and convenient location. The parcels are well suited to accommodate a retail store area and the parking, storage warehouses, truck loading and service areas these businesses require.

With the planned improvements to the Boulevard (i.e. streetscape landscaping, signage, and lighting) there is an opportunity for expansion of home improvement related businesses.

The design standards for the SPAO will support unique building designs that contribute to the desired destination character and landmark stature of the Boulevard.

Leisure, Entertainment and Recreation Uses

West Auburn Boulevard is well located to provide a broad range of leisure and recreation oriented businesses. Examples include: restaurants and meeting facilities, entertainment centers, fitness centers, sporting goods sales and service.

The high traffic volumes along the freeway, the ready access from a large market population within six miles, and proximity to well-established neighborhoods indicate a potential for businesses with a regional draw.

The nearby Haggin Oaks Golf Courses, Del Paso Park and the regional softball complex, and Cal Expo suggest that the Boulevard could accommodate recreation themed businesses, restaurants and meeting facilities that would complement the events and activities at these nearby facilities.

The freeway location makes the Boulevard a convenient place to stop on the way to and from work, perhaps to visit a gymnasium or sports facility.

A few notable entertainment, music, dance, and leisure-oriented shops and services already occupy the Boulevard. The addition of new entertainment facilities, such as restaurants, dinner clubs and comedy clubs could establish the Boulevard as a regional entertainment district.

These land uses could also host special events such as dances, music festivals, art and craft festivals, and other similar gatherings.

Neighborhood Retail and Services

The Arden-Arcade community includes a number of commercial-core areas that serve residents. The West Auburn Boulevard corridor is located at the very north edge of the community and therefore is not central to most of the residents. However, the residential areas immediately to





The Cluster use type can include taller buildings in groups that define common open space, such as plazas.

the south of the Plan area are not well served by neighborhood retail and services. Moreover, there are few locations within or near these neighborhoods that can provide space for such services or retail without displacing an existing business.

Appropriate businesses would include: restaurants, small retail stores, and a variety of personal and professional services.

Cluster Development

The SPAO encourages the design of mixed-use blocks in which smaller shops and offices are clustered in the center and toward the front of the parcels.

The properties along the Boulevard are generally not suited to small businesses because the properties are too deep to be fully utilized by small businesses.

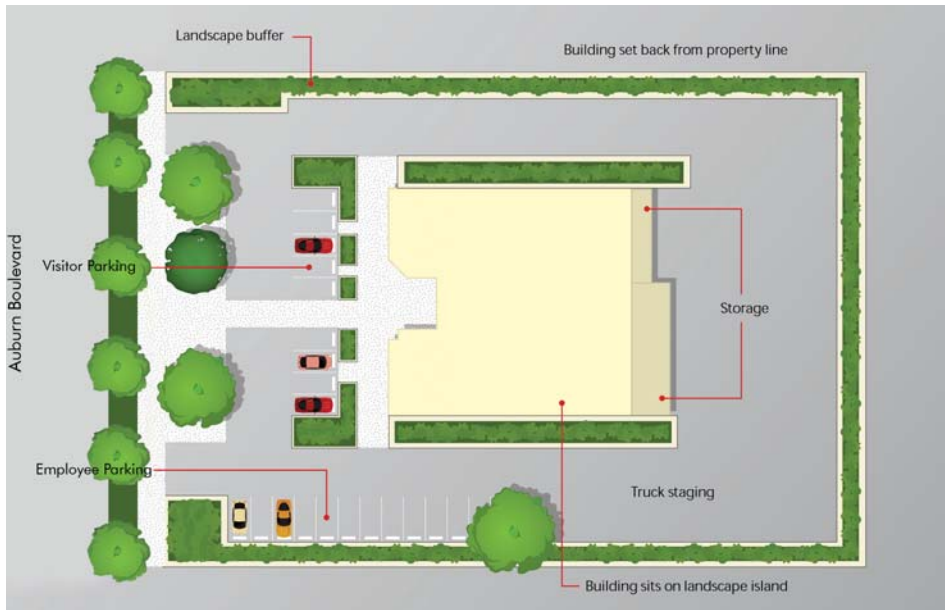
Efficient development of these deep and narrow lots requires a strategy to allow for more width in the core of the parcel. Commercial uses naturally tend to maximize their frontage exposure to passing traffic, but many of the parcels are relatively narrow and the full depth of the parcel is not efficiently utilized.

One strategy is to aggregate them into configurations that approximate at least a 2:1 depth to width ratio and preferably closer to a 1:1 ratio. Once aggregated, the lots can be developed in a single large use oriented to the street or may be developed in multiple uses that penetrate the rear of the lot as well as provide street front orientation.

Another strategy is to allow shared parking and shared driveways that cross from one parcel to another near the middle or rear of the parcels.



Example of Cluster Development



Example of Small Front Retail

Building facades along the Boulevard may be opened to provide access to uses in the interior of a parcel. Smaller, pedestrian-scale buildings would be located in the center and rear of the parcel to fully utilize the site and extend to the street to provide a visual and pedestrian connection from the sidewalk along the Boulevard. The cluster building activity type is intended to connect to similar parcels to either side and thereby effectively increase the interior width of the narrow parcels. Interconnecting driveways between parking areas, joint parking areas, and interconnecting pedestrian paths are strongly encouraged.

These “pedestrian enclaves” would provide the smaller scale uses, such as, restaurants, smaller retail stores, services and offices. These could include uses that have a community-wide draw, but are particularly suited to uses that serve the local neighborhood.

Small Front Retail

The small front building type includes those with less than 150’ of frontage oriented to the street. Small front retail is the most common single-use building type currently located on the Boulevard. Many existing parcels have a street frontage of 110’ or less. Aggregation of these narrower parcels will be difficult due to recent development adjacent to them. The challenge with these buildings is to make the most use of a deep lot that may have more land than is needed.

Where the cluster building type is not practical, the site can be developed as a small front building. In these cases the building would present a conventional frontage to the Boulevard, but would include parking areas and building fronts oriented to the side or rear property boundary.

Multi-Family and Mixed-Use Residential

Multi-family housing in this Plan area would be attractive to seniors and young professionals who appreciate the proximity to downtown jobs, the urban lifestyle, the access to public transit (including light rail), the view across the freeway to the Haggin Oaks Golf Courses, and the proximity



to recreation opportunities. Ultimately, the Boulevard will become known as a lively place to live if the following objectives are fulfilled; high design standards, a mix of entertainment, recreation and leisure activities.

The multi-family residential type would be stand alone apartment buildings or integrated in a mixed use building with office and/or commercial uses. In most cases it is likely that multi-family residential uses would be located adjacent to non-residential uses. Providing private space for recreation and leisure within the building type must be a major design consideration.

The integration of multi-family residential use will require careful design to ensure the peace and privacy of the occupants and for the existing residents to the south. Design of residential uses will require mitigation of freeway noise and screening of views into the adjacent residential areas.

E. Permitted Land Uses

The West Auburn Boulevard SPAO shall apply to all land uses envisioned in this Plan.

The table of Permitted Land Uses summarizes the permitted (P) and conditionally permitted (CUP) uses in the SPAO. Uses not included in the table of Permitted Land Uses shall be considered to be prohibited in the Plan area. The Planning Director has discretion as to application of the guidelines relative to allowed versus non-allowed uses.

Specific standards that regulate hours of operations, noise, and light and truck traffic are assigned to certain land uses. Notations in table of Permitted Land Uses indicate the specific considerations or conditions that may apply to certain uses. These are described in footnotes following the table of Permitted Land Uses.

Note: If there is a use in the table that is missing, refer to Section 225-11 in the Sacramento County Zoning Code and refer to the uses that are allowed in the Limited Commercial (LC) Zone for all of the properties within this SPA.

Table
Permitted Land Uses Footnotes per page 3.20

A. AUTOMOTIVE SALES, SERVICE, REPAIR

1.	Auto Sales, Auto Broker, New/Used	1, 2, 3, 4, 5, 6,12
2.	Auto Rental or Lease Agency, Including Limousine Service	
3.	Auto, Service – Repair – Replacement (Not a part of Auto Sales, New)	
4.	Auto Service Station, Primary	CUP
5.	Auto Parts and Accessory Store	
6.	Auto Wash, Self-Service or Automatic	7
7.	Camper Shell – Sales, Repair, Rent	
8.	Motorcycle, Sports Cycles, Trail Bikes, Jet Skis, Snowmobile, Ultra-Light, Moped – Sales, Rent, Service, Repair	8
9.	Gasoline Sales and Service Station	CUP

<i>Table</i> Permitted Land Uses	Footnotes per page 3.20
-------------------------------------	----------------------------

- B. BUSINESS SERVICES**
- 1. Advertising Business
- 2. Blueprinting-Photostatting Service
- 3. Computer Programming/Software and System Design
- 4. Computer Sales, Rental and Lease
- 5. Computer Service and Training
- 6. Data Processing Service
- 7. Delivery Service 9
- 8. Drafting Service
- 9. Furniture Rental Agency
- 10. Janitorial Service 9
- 11. Locksmith – Safe Repair Shop
- 12. Messenger Service
- 13. Office Machines and Equipment Sales and Minor Repair
- 14. Photocopy Service
- 15. Print Shop
- 16. Remote Teller, Freestanding for Pedestrian Use
- 17. Stenographic Service
- 18. Studio – Radio, Television, Recording
- 19.. Ticket Agency
- C. HEALTH SERVICES**
- 1. Clinic, Child Family Guidance
- 2. Clinic, Counseling
- 3. Clinic, Diet Counseling with Incidental Sales of Diet Products
- 4. Clinic; Kidney Dialysis
- 5. Clinic, Physical Therapy
- 6. Eyeglasses, Frames, Contact Lens – Sales and Service
- 7. Hearing Aids – Sales and Service
- 8. Laboratory – Medical, Dental or Optical
- 9. Medical or Dental Office
- 10. Orthopedic Appliances Sales/Service
- D. PERSONAL SERVICES**
- 1. Barber Shop
- 2. Beauty Shop and Wig Sales
- 3. Child Care Center
- 4. Dressmaker / Tailor



<i>Table</i>		Footnotes per
Permitted Land Uses		page 3.20
5.	Reducing-Body Building/Aerobics Studio	7
6.	Shoe Shine Parlor	
7.	Studio – Dance, Voice, Music, Gymnastics	7, 10
8.	Social Center	10
E. MISCELLANEOUS SERVICES		
1.	Laundromat, Self-Service	10
2.	Laundry or Cleaning Agency, Retail (On-Site Cleaning Permitted)	
3.	Parking Lot or Garage as Primary Use	
4.	Photography Studio, Including Incidental Processing	
5.	Picture Framing Shop	
6.	Travel Agency	
F. REPAIR SERVICES (SEE SECTION A FOR AUTO REPAIR)		
1.	Appliance Repair Shop	
2.	Electronic Equipment Repair	
3.	Shoe Repair Shop	
G. EATING/DRINKING/LODGING		
1.	Soda Fountain-Ice Cream Parlor	
2.	Restaurant-Coffee Shop-Cafeteria	
3.	Bakery, Pastry Shop	
4.	Bar-Tavern	10, 11
5.	Brew Pub (No Wholesale or Off-Site Sale of Beer, Wine, or Alcohol)	10, 11
6.	Catering Service	
7.	Delicatessen	
8.	Hotel	CUP
9.	Motel	CUP
H. ENTERTAINMENT/RECREATION SERVICES		
1.	Arcade – Electronic, Mechanical or Video Games	CUP
2.	Art Galley	10
3.	Art Studio	
4.	Dance Hall-Ballroom-Discotheque	10, 11
5.	Dancing as an Incidental Use in a Bar or Restaurant	10, 11
6.	Library	
7.	Live Dinner Theater	
8.	Motion Picture Theater	10
9.	Museum	

<i>Table</i>		Footnotes per
Permitted Land Uses		page 3.20
10.	Physical Fitness Studio	
11.	Recreation Facility, Indoor	
12.	Recreation Facility, Outdoor	
I. FOOD, DRUG, LIQUOR SALES		
1.	Bakery, Pastry Shop	
2.	Candy Store	
3.	Certified Farmer's Market	
4.	Convenience Store/Neighborhood Market (Less than 6,000 square feet in size)	7
5.	Nonprescription Drugs and Sundries	
6.	Food Market Ancillary to Service Station	CUP
7.	Prescription Pharmacy	
8.	Supermarket-Food Store	7
J. GENERAL MERCHANDISE SALES		
1.	Excludes prohibited uses listed in footnotes, per page 3.21	13
K. HOME ACCESSORIES AND SERVICES		
1.	Antique Store	
2.	Appliance Store	
3.	Floor Covering, Drapery or Upholstery Store	
4.	Furniture Cleaning, Refinishing, Re-upholstery Shop	
5.	Upholstery Shop (no refinishing)	
6.	Furniture Store	
7.	Gardening-Landscape Supply Store	
8.	Interior Decorators Service Yard and Workshop	
9.	Paint and Wallpaper Store	
10.	General Glass Sales, Services	
L. RECREATION EQUIPMENT SALES		
1.	Athletic Equipment and Sporting Goods	
2.	Bicycle Sales, Rent, Service	
3.	Golf Cart – Sales, Repair, Rent	
4.	Marine Supply and Boat Sales Store	1, 2, 3, 4, 5, 6
5.	Saddlery Shop	
6.	Tackle Shop	
O. OFFICES		
P. PUBLIC FACILITIES		
1.	Privately-Owned Uses within Public- and Government-Owned Buildings, Facilities and Groups	



<i>Table</i> Permitted Land Uses		Footnotes per page 3.20
2.	Building and Facility owned by Federal and State Governments, and located on Federal and State owned property	
3.	Public- and Government-Owned Buildings and Facilities Other than Federal and State	
4.	Public and Government Uses, Other than Federal and State, within Privately-Owned Buildings, Facilities and Grounds	
5.	Public Utility and Public Service Facility	
R. RESIDENTIAL		
1.	Apartment-Multiple Family Dwelling, Condominium	
2.	Home Occupation	
3.	Residence of a Caretaker, Proprietor or Owner of a Permitted Use	
4.	Residential Care Homes for Adults or Children	
S. SCHOOLS		7
1.	Business School	7
2.	Charm, Culture School	
3.	College and University	
4.	Driving School	7
6.	Self-Defense, Judo, Boxing, Gymnastics, Swimming or Similar Activity	
T. TRANSPORTATION FACILITIES AND SERVICES		
1.	Parking Lot/Garage (Primary Use)	

Table of Permitted Land Use Special Conditions: Operation Requirements for Permitted Uses

Footnotes: The permitted uses listed in Table 3-2 must comply with the following conditions where noted in the numbered items in the "Specific Conditions" column.

1. *The permitted use shall include a permanent building that conforms to the design regulations in Section Four of this Plan. The permanent building shall include a showroom for display of for-sale merchandize and a business office.*
2. *The permitted use shall have a minimum parcel size of 1.5 acres.*
3. *No outdoor public address or loud speaker shall be permitted.*
4. *Service areas shall not operate between the hours of 10 PM and 7 AM.*
5. *Sales area lighting located within 100 feet of a residential zoned property shall not be operated between the hours of 10 PM and 7 AM.*
6. *Auto and truck access doors to service bays, tire shops, machine shops or other areas where machinery is operated shall not be oriented toward a residential-zoned property unless the noise level at the adjacent residential-zoned property line would not exceed the County noise level standard. Noise levels may be mitigated through various methods including, but not limited to, sound baffles around equipment and sound*

walls. A sound study shall be required to demonstrate that the proposed mitigation will meet the County noise level standard.

7. To ensure compliance with General Plan Noise Element standards of 65 dB Ldn or less for residential/ transient lodging outdoor activity areas and 45 dB Ldn or less for residential/ transient lodging interiors, the following measures shall apply:
 - a. Any/all common outdoor activity areas shall be designed and attenuated such that after all applied shielding or buffering mechanisms noise levels are 65 dB Ldn or less.
 - b. Any/all new residential/transient lodging construction shall be designed and attenuated such that the interior noise level does not exceed 45 dB Ldn.

The above measures must be accompanied by an acoustical analysis, prepared by a qualified acoustical consultant and verified by the Department of Environmental Review and Assessment, substantiating that the General Plan Noise Element standards cited above will be met.

8. Operation shall not be permitted between the hours of 10 PM and 7 AM.
9. Service operations are permitted only in conjunction with a sales operation that is the primary business.
10. Delivery and service vehicles must be secured in an enclosed garage or fenced yard during non-business hours.
11. Hours of operation shall not be permitted with the hours of 12 AM and 10 AM.
12. Private security shall be provided by the operator if regulated by the County Sheriff.
13. Auto uses are not allowed in Neighborhood Connectivity Zones.
14. Auto uses are prohibited in Neighborhood Transition Zones.
15. Any uses not listed in the Table of Permitted Land Uses are prohibited, including, tattoo parlors, adult businesses, cigarette sales, pawn shops and liquor or tobacco stores.

F. Vacant and Under-Utilized Parcels (Transitional Uses)

Properties that are currently vacant, mobile home parks and older motel, office and industrial uses that are in poor condition, occupy a total of approximately 45 percent of the land in the Plan area. These are considered transitional land uses that would be redeveloped into more appropriate, higher valued uses over time.

These transitional uses and vacant sites are scattered along the Boulevard, but there are clusters at the intersection of Fulton Avenue, Howe Avenue and Morse Avenue that suggest opportunities for redevelopment as significant stand alone uses, or as clusters consistent with the urban form zones described in this Plan.

All transitional uses would continue to operate under the current zone classification that applies to the property. However, the new zone designation "West Auburn Boulevard SPAO" would consider these as non-conforming uses. Cessation of the current use of the property or destruction of the property improvements would require that future uses would conform to the SPAO.

Some of the existing transitional uses are now in deteriorating condition and would not attract the reinvestment necessary to participate in the future of the Boulevard. With the lack of reinvestment these uses would continue to decline and ultimately be demolished. The economic value

of such uses may continue for some time. It is assumed that many of the older buildings are fully amortized and the cost of continued operation is nominal compared to the available rents. However, over time a combination of rising land value and continuing deterioration of the physical improvements will result in the inevitable conversion to a higher and better use. Consequently, existing uses that do not fit in the future vision of the Boulevard are considered transitional.

In the transitional period it is typically not necessary to initiate a change in land use. It is sufficient to establish the vision and zoning framework for future development and then allow market forces to effect the change. However, it is possible that implementation of a specific development concept would require the acquisition and conversion of a specific site to a future use. In such cases the processes of acquisition and consolidation by the Sacramento Housing and Redevelopment Agency may be appropriate.

G. Design Principles and Strategies

Development standards are guided by design principles and strategies related to the SPAO goals and objectives for economic development, overall quality in design, and creation of a landmark district.

Optimal Use of the Available Land

Developable land along the freeway with visibility, access, market potential and substantial public investments in infrastructure and services is a limited resource. Such resources should be developed to the maximum extent feasible.

High Quality Design and Variety Within a Distinct Design Framework

The Plan area will be developed over a period of time by different developers. Each will propose a design that suits the individual objectives, tenant requirements, economic conditions and aesthetic sensibilities of the project proponents. Yet, it is highly desirable that the total Plan area reflect common design elements to enhance the sense that this is a “special place” with a distinct identity

High quality design within a common design framework will establish community identity and sense of place without a strict adherence to a particular style. The design framework is established by common building elements and forms, response to the climactic conditions, the linear form of the street, colors and materials, and in the streetscape landscaping, lighting and signage specified in the Streetscape Master Plan.

Identify Potential Landmark Sites

Certain key properties can be used to establish the identity of the Plan area by developing landmark buildings or other notable features, such as a signature landscape feature, special lighting that highlights the building, memorable signage, a sculpture or a mural visible from the street. These landmark properties will be located at primary intersections and/or where they will be clearly visible from the freeway.

The potential locations of landmark blade signs are identified in the Streetscape Master Plan. The design review of any proposed new development or redevelopment on these properties will consider the landmark potential inherent in the site. Inducements such as modification of parking requirements or modification of the signage requirements may be considered to encourage the design of attractive and notable landmark features.

Sustainable/Energy Conserving Design

Energy conserving design will not only reduce the on-going operating costs of buildings but will also minimize the demand for new energy sources. Moreover, the design of energy conserving buildings will inherently reflect the climatic conditions of this region and thereby help establish a distinctive architectural style. Sustainable design that responds to the local climate will naturally generate architecture with similar forms and articulation.

West Auburn Boulevard dips at a uniform angle of 27 degrees south from an east-west axis. Consequently, the rear of all buildings on the Boulevard have excellent solar exposure to the southeast. However, the face of the buildings will be exposed to late afternoon sun in the summer. Conventional approaches to shading, such as screens or placement of large shade trees would conflict with the sales exposure to the freeway desired by many retailers. Use of innovative means of shading the west facing glass could provide a distinctive architectural feature that would help create an identity for the Boulevard. Examples include moveable shades, strategically placed columns, strategically located trees or wing walls that would shade but still allow views to the display windows.

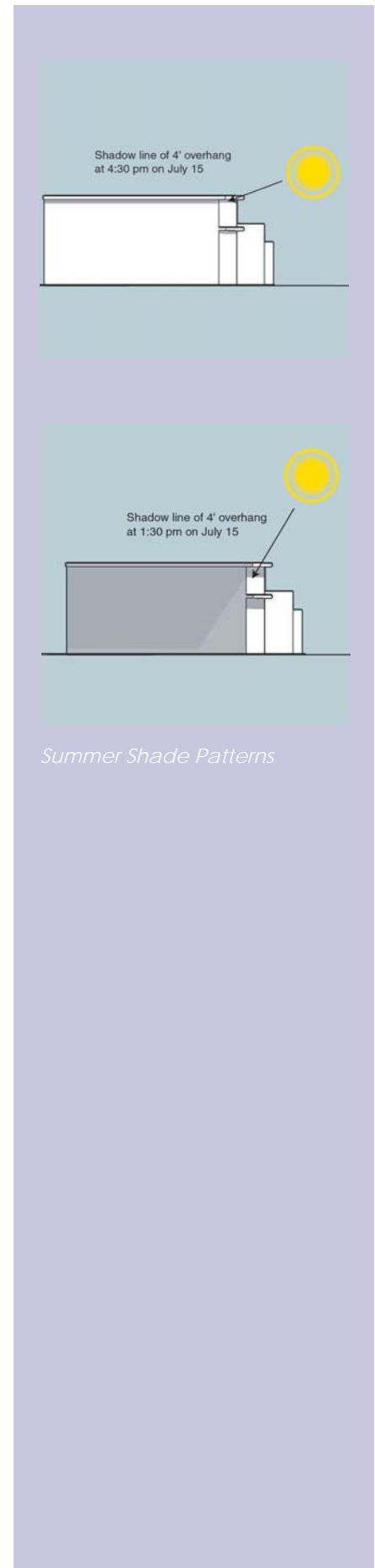
Buildings and site improvements are to be designed with sustainability as a primary consideration. Sustainability requires, first, durability and resource conserving character of materials used in the project and, second, the use of energy conserving design of building and water conserving design of landscaping. The sustainable character and features of the building and site design should be apparent and made part of the presentation for project approval. It is strongly recommended that all buildings be designed to comply with LEED (Leadership in Energy and Environmental Design) certification standards.

Establish Attractive Connection to Haggin Oaks Golf Courses

The Haggin Oaks golf courses add a special amenity to the Plan area both visually and as complementary uses. Currently, the advantage of the proximity of the courses to the Boulevard is negated by the unattractive and outdated connection on the Fulton Avenue overcrossing.

The Plan area does not include the over crossing or any of the facilities on the north side of the freeway. However, the Plan area should facilitate future improvements to the overcrossing by requiring that the design of any public improvements in the vicinity of the Fulton Avenue intersection be related to the golf courses, perhaps with signage and/or other design elements.

Establish Enhanced and Attractive Pedestrian Connection to the



Watt Avenue Light Rail Station

The light rail station is located approximately one-half mile north of the Plan area. The current uses in and around the Plan area do not have a strong relationship to the station. Nonetheless, access to the light rail station should be enhanced wherever the opportunity exists. In this Plan area, future redevelopment of the area around the Watt Avenue intersection could provide a nucleus of transit-supported businesses and high density residential uses.

The Plan area should provide an enhanced pedestrian connection on the west side of Watt Avenue and the south side of the Boulevard to improve access to the light rail station.

The design review of any proposed new development within one-quarter mile of the Watt Avenue/West Auburn Boulevard intersection should consider high intensity uses. Inducements, such as, reduction of parking requirements or modification of the signage requirements may be considered to encourage the highest intensity of mixed use development that can be accommodated.

Any use of public funds for public improvements or support of redevelopment of these sites will require development to be of sufficient density to support public transit.

Establish Pedestrian Connections to Adjacent Neighborhoods

The minor streets (Howe Avenue, Bell Street and Morse Avenue) that intersect the Boulevard shall have strong pedestrian connections to the adjacent neighborhood. This shall include adequate sidewalks, shade trees and pedestrian-scale lighting on that portion of the street that connects the Boulevard to the neighborhood.

H. Urban Form Zones

The West Auburn Boulevard Plan Area includes distinctly different sub-areas or “urban form zones.” These urban form zones are determined by many factors, most notably the physical setting, the adjacent land use, exposure to the freeway, local traffic, and existing land use.

Although the Special Plan Area Ordinance allows a mix of land uses to occur at any location along the Boulevard, there are some uses better suited to certain zones. These factors suggest that the land use and development standards should be based on different “urban form zones” along the Boulevard.

These zones are described in the following sections and are identified in Figure 3-3.

Watt Avenue Zone

The intersection of Watt Avenue and Auburn Boulevard will remain a major regional transportation hub. Watt Avenue will continue to grow in regional significance as a major north-south corridor linking the Highway 50 corridor to I-80 and ultimately to the Placer Parkway near Roseville.



Urban Form Zones

As a major crossroads in the region, and located near the Watt Avenue Light Rail Station, the Watt Avenue/Auburn Boulevard intersection has the potential to develop as a high intensity core area. The Watt Avenue Zone would ultimately develop as a mix of office and high density residential uses. The intersection is also a primary gateway to the Arden-Arcade community and the commercial use core at Watt Avenue and El Camino Avenue. Future uses should take advantage of the transportation opportunities and provide a notable landmark gateway to the community to the south.

The development of this area should include taller mixed-use buildings that provide a landmark anchor for the West Auburn Boulevard corridor. Future development should be notable for its landscaping, signage and architecture.

Fulton Avenue Zone

The intersection of Fulton Avenue and Auburn Boulevard offers a unique opportunity to provide a gateway to the Arden-Arcade community and a link to the golf courses to the north. The intersection is the geographical center of the Plan area and should be developed as a “center piece” that links the West Auburn Boulevard Streetscape Master Plan improvements to the Fulton Avenue corridor.

Future development at this intersection should provide a visual connection to the existing design theme along Fulton Avenue to the south. Buildings should be taller than the adjacent uses and should provide landscaping and signage that relates to the overall design theme for the Boulevard.

Freeway Visibility Zones

Certain locations along the Boulevard have much more direct exposure to the freeway and are much more suited to uses that benefit from high visibility. Retail uses are most likely, but certain other services and office uses would also select a high visibility location, if offered a choice. Land uses that benefit from high visibility and are less sensitive to freeway noise would be located in areas where the line of sight from the freeway is superior.

The development standards in these areas will favor high profile uses that benefit from freeway exposure. Buildings need not be tall, but will incorporate attractive design features that provide a pleasing impression of the West Auburn Boulevard district from the freeway.

Neighborhood Connectivity Zones

The adjacent residential neighborhoods to the south have very little relation to the businesses along the Boulevard due in part to the lack of mid-block connectivity. The existing neighborhood-serving uses tend to be clustered near the major street intersections (Watt Avenue and Fulton Avenue), but these are generally auto-oriented uses that do not favor pedestrians. The only notable exception is the small convenience retail outlet at Morse Avenue and the nearby Deseret Industries store.

One of the goals for this SPAO is to provide more connectivity to the adjacent neighborhood to encourage residents to walk to local retail and services. The development standards encourage development of pedestrian friendly, local service land uses along the Boulevard in the vicinity of Bell Street, Howe Avenue and Morse Avenue.

Higher intensity uses that would support and benefit from public transportation will be clustered within walking distance of existing public transit service. Specifically, Regional Transit bus route 26 and the Watt Avenue Light Rail Station. The bus stops would be enhanced with shelters and improved access to the north side of the Boulevard for south-bound buses.

SECTION FOUR: Development Regulations

Section Four details the Development Regulations including architectural standards, building size and proportion, building orientation, pedestrian paths, public transit access, security, driveways, parking, landscaping, lighting, signage and various other considerations.

A. Application of the Development Regulations

Applicants for new development or improvements to existing structures in the Plan area will apply the development standards described in this section. The SPAO is intended to be comprehensive and to replace the current zoning and land use regulations within this Plan area.

Where the SPAO is silent, the County's Zoning Ordinance will apply.

The standards describe and provide for variations that allow for location differences (the urban form zones) and different building use types. All development standards will apply unless specifically noted as a special case that applies only to a location or a building use type.

B. Architectural Standards

Coordinated Design

- All exterior facade elements will reflect a coordinated design concept, including expression of building function, structure and scale;
- The use of materials, color and finishes will be coordinated to achieve a sense of continuity and quality in design.

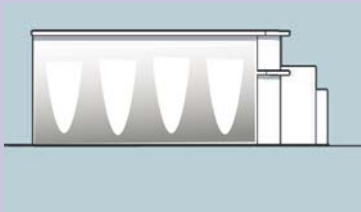
Building Materials

These design standards do not specifically limit the form or application of exterior finish materials and creative use of materials is encouraged. The intent is to create a Boulevard district that is notable for quality and contemporary design.

- Exterior materials will convey permanence and substance. Plastic materials meant to represent a natural material, such as stone or wood, are discouraged.
- The appropriate exterior materials include metal, glass, masonry, plaster, stucco, stone and decorative treatments in concrete or asphalt.
- Brick and concrete block used in traditional styles are inconsistent with the vision of a contemporary design district.

Colors

The use of "appropriate" color is highly subjective. The intent is to avoid large panels of primary colors that demand attention.



Accent Lighting

Lighting washes on a plain wall can create distinctive forms.

- Bright colors and primary colors are acceptable as accent colors. Primary colors should only be used to accent elements, such as, door and window frames and architectural details.
- Large areas of intense white color should be avoided.
- The color palette chosen for new structures should be compatible with the color palette established in the Streetscape Master Plan.
- Architectural detailing should be painted to complement the facade and tie in with adjacent features.

Architectural Lighting

Architectural lighting that provides drama and interest to buildings is consistent with the SPAO goal to create a landmark district. Consistent use of simple techniques or common elements, such as repetition of the lighting design applied to the landmark signage in the Streetscape Master Plan would establish a distinctive design character for the entire district.

- Consideration of both interior and exterior lighting shall be evident in the design of buildings fronting on the Boulevard.
- External night lighting will be used to enhance and articulate the buildings without glare directed off-site.
- Ground based up-lighting that washes the primary walls or highlights architectural features or detailing will be used on the facade fronting on the Boulevard.
- Lighting fixtures will not project above the fascia or roof line of the building and are to be shielded. The shields will be painted to match the surface to which they are attached. Security lighting fixtures are not to be substituted for parking lot, drive aisle, or walkway lighting fixtures, and are not restricted to lighting only loading and storage locations or other similar service areas.
- Wallpack lighting is discouraged on all facades facing the Boulevard or publicly accessible areas.

Energy Conserving Design

Energy conservation is implemented through building and landscape design, as well as the building orientation compatible with the climatic conditions.

- Passive solar design is encouraged whenever possible. Design of buildings will demonstrate consideration of energy-efficient concepts, such as natural heating and/or cooling, sun and wind exposure and orientation, and other solar energy opportunities.
- Natural lighting is encouraged.
- Life-cycle costs of buildings will be considered in all buildings.
- Use of thermal mass to moderate the heating and cooling of structures and public spaces will be considered in the design of all buildings.
- Application of the Leadership in Energy and Environmental Design (LEED) Green Building Rating System is encouraged.
- Solar collectors, if used, will be oriented away from public view or designed as an integral element of the roof structure.
- Sun shade structures such as building overhangs, verandas, trellises and porticoes will be incorporated in the design of all buildings at the primary entry and pedestrian approaches to all buildings.

Architectural Detailing

- Architectural elements that contribute to a building's character, aid in climate control and enhance pedestrian scale are encouraged. Examples include: canopies, roof overhangs, projections or recessions of stories, balconies, reveals and awnings.
- Building entry ways will be clearly defined and integrated with building and landscape design. The use of distinctive architectural elements and materials to denote prominent entrances is required.
- Doors and windows will be consistent in design and located to present a unified appearance to the elevation except where the variations are an integral and necessary part of the exterior design.
- Simulated or blacked out windows are strongly discouraged.
- Buildings will incorporate detailing where it is discernible by pedestrians and motorists viewing the building from typical locations. Pedestrian scale features such as porticoes, arbors and promenades are particularly important.
- Walls adjacent to a walkway will include windows, trellises, wall articulation, wainscot, arcades, changes in materials or other features to ensure visual diversity and proper scale. Architectural detailing of each wall at ground level will relate to the landscape to ensure an appropriate transition of the building and the ground plane.
- Retail shops and offices fewer than 10,000-square-feet per tenant will include a minimum of 15 percent window front length adjacent to walkways along the ground floor wall. These windows will not be covered inside the building and will allow direct line-of-sight into the building from the nearest parking area or driveway.
- All roof top equipment must be screened from public view using materials of the same nature as the main structure. Mechanical equipment should be located below the highest vertical element of the building.

C. Street Presence/Mass and Scale

Street presence refers to how the building is perceived from the street. It encompasses the issues of building height, form, massing, detailing and articulation.

Building Height

The scale of buildings along the Boulevard should be grand rather than intimate. The Boulevard will include a pedestrian corridor along the south to side of the street, but the scale of buildings and landscaping will generally give priority to the view from a vehicle on the street. The perception of building scale differs greatly when one is a pedestrian a few feet from the building or a motorist on the freeway traveling at 50 mph.

- Building height will be regulated by the distance from the rear property line and by the distance from the intersections at Bell Street, Morse, Howe, Watt and Fulton Avenues. Buildings should be taller at the major streets (Watt and Fulton) to signal significant gateway landmarks. Conversely, the buildings should be lower at the intersections with streets that lead into the residential neighborhoods to present a pedestrian scale connection.



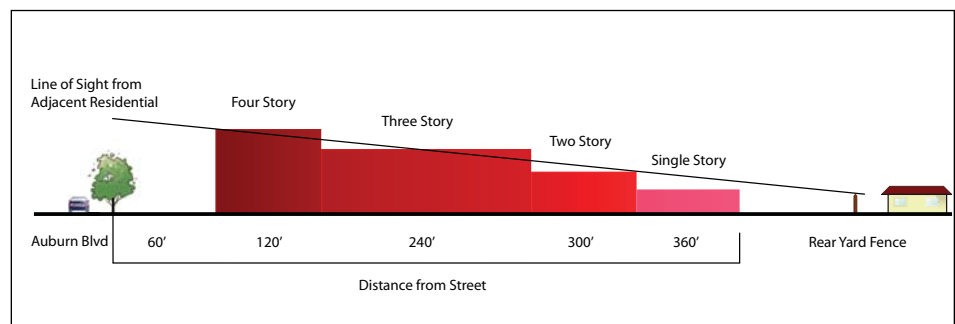
Simple banding and detailing can create visual interest on the simplest building.



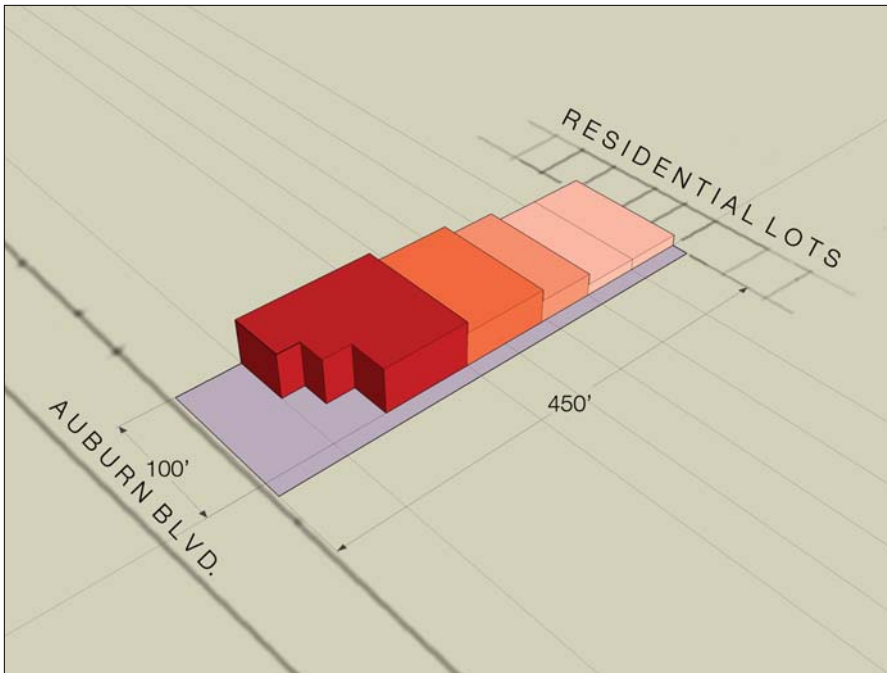
- Buildings adjacent to existing residential neighborhoods should that step down toward the rear to provide privacy for the existing residences, and to rise up near the major intersections to provide a landmark feature. Buildings may be up to four stories and 50' high near the Boulevard, but will step down in height toward the rear so that no portion of a building may be more than two stories high within 50' of the rear property line
- Heights of structures should allow maximum natural light and ventilation, protection from prevailing winds, enhance public views and minimize obstruction of view from adjoining structures.
- Height and scale of new development should provide a "transition" from the height of the lowest adjacent development to the maximum height of the proposed structure.
- In each cluster building use type a single building or portion of a building not to exceed 400-square-feet in area may extend up to 50' in height. The building may be used as a clock tower, bell tower or other similar purpose. The intent is to provide a visual focal point near the common area.

Building Heights Permitted			
Distance from	Distance (feet)	Stories	Height (feet)
Watt Avenue*	500'	6	70'
Fulton Avenue*	500'	6	70'
Bell Street, Howe and Morse Avenues	200'	2	25'
Rear Property Line	50'	1	15'
Rear Property Line	100'	2	25'
Rear Property Line	150'	3	40'
Rear Property Line	250'	4	50'

* Height allowed relative to Watt Avenue and Fulton Avenue shall supersede height allowed from rear property line.



Proposed Permitted Building Height Section

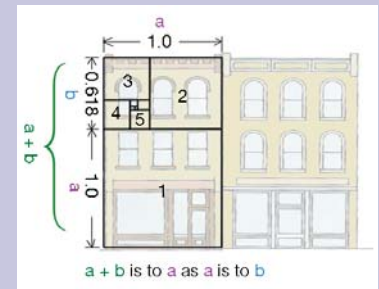


Proposed Permitted Building Heights - Axonometric

Building height would increase from rear toward the front of a typical lot.

Building Mass and Proportions

- Architectural elevations should employ the “Golden Proportion” in the proportion of major building elements, window and door frames, columns or other vertical elements. Multiple uses of the Golden Proportion in several buildings along the Boulevard will create a subtle sense of continuity despite different building materials and styles.
- Large dominating structures should be broken up by: 1) creating horizontal emphasis through the use of trim; 2) adding awnings, eaves, windows, or architectural ornamentation; 3) use of combinations of complementary colors; and 4) landscape materials.
- Large expanses of walls will be articulated with a combination of small and well defined sections, architectural detailing, color and/or composition of facade elements.
- Wall surfaces in buildings less than 50’ in height will avoid running in one continuous direction greater than 100’ without an articulation feature such as a pilaster, offset, or change of color or material.
- Multi-story buildings should have a window line on the upper floors. Notably, retail stores, such as furniture stores are encouraged to use the upper floor window areas for display of the offered goods.
- The roof line at the top of the structure should incorporate offsets and jogs to reduce the monotony of an uninterrupted roof plane.

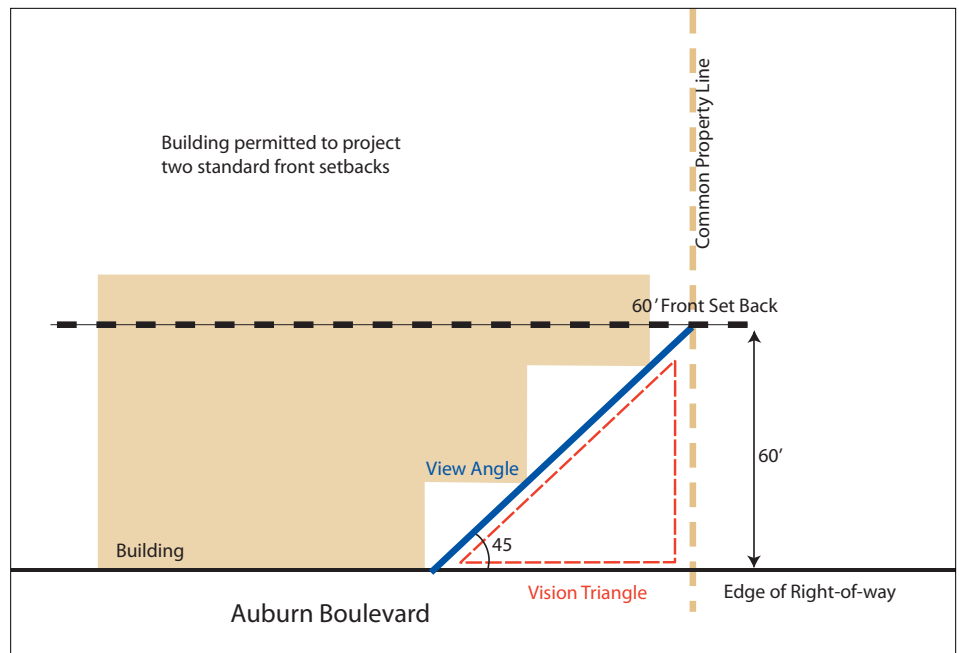


Golden Ratio

D. Relation to the Street (Front Setbacks)

The general intent is to establish a building setback from the street to provide adequate space for a landscape and parking area and/or a display of vehicles for sale. The standard setback would ensure that no building would block the view of the neighboring building or vehicle display area. However, variation in street setback would provide visual interest to the streetscape and allow better use of the site for smaller buildings.

The front setback for all buildings will be established by a vision triangle measured relative to the side property line and the back of side walk. The front setback will be defined by a line located at a 45-degree angle to the side property line beginning at a point 60' along the side property line measured from the back of the sidewalk. However, a building may be located within the front setback area provided that the front setback line at the edge of the adjacent property is visible at a distance of 200' from the side yard property line.

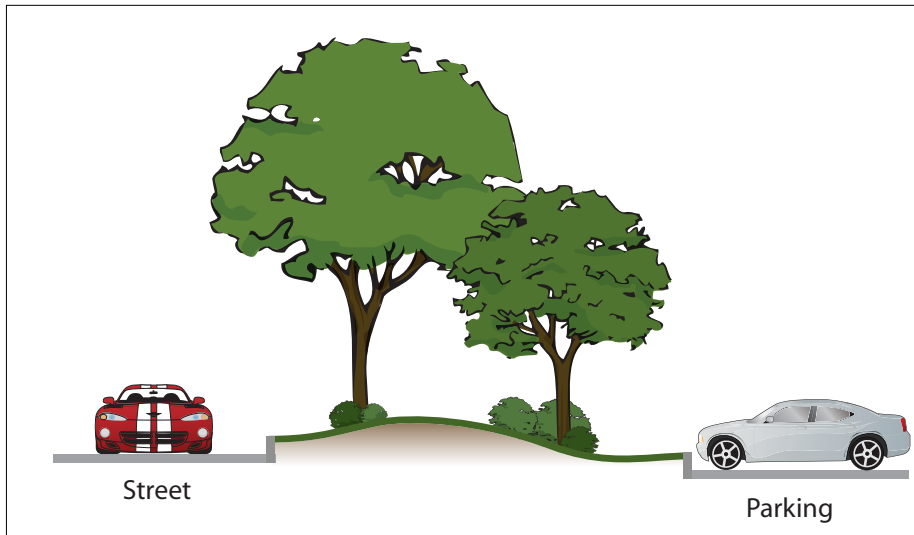


Proposed Front Setbacks (Plan View)

E. Streetscape and On-Site Landscape Interface

The front setback area may include a public improvement, such as a sign or light standard that is part of the streetscape. The location and dimensions of these improvements is specified in the Streetscape Master Plan. The County will have the right to acquire the land for these improvements and expand the public right-of-way.

Autos for sale may be placed on a raised platform or on a berm that is not more than four feet higher than the adjacent parking area. Such berms or platforms will not occupy more than 20' of any single dealership frontage and shall be constructed of materials consistent with these regulations and streetscape materials.



Proposed Landscape Buffer

Relation to Adjacent Use (Rear Yard Setbacks)

The rear property line abuts a residential use in virtually all cases. The rear property line standard is intended to provide privacy for the adjacent residential uses and security for both the residential and non-residential uses.

The existing conditions include buildings located at the rear property line on many of the lots. A utility line runs along the rear property line of most locations. This precludes the use of trees to screen the adjacent use. Therefore, the rear property line of all properties will provide an eight foot high masonry wall. A planting strip not less than eight feet wide will be provided along the base of the wall. The plant materials should include low shrubs and climbing plants to screen the wall. Trees should be planted where possible.

Relation to Adjacent Use (Side Yard Setbacks)

All non-residential buildings have no side yard setback requirement. However, each developed parcel will provide a minimum 20' wide fire access corridor extending from the front property line to within 100' of the rear property line. The fire access may be a driveway or landscaped area and may be located at any point along the property frontage including the side yard.

Residential uses will provide a minimum 10' side yard.

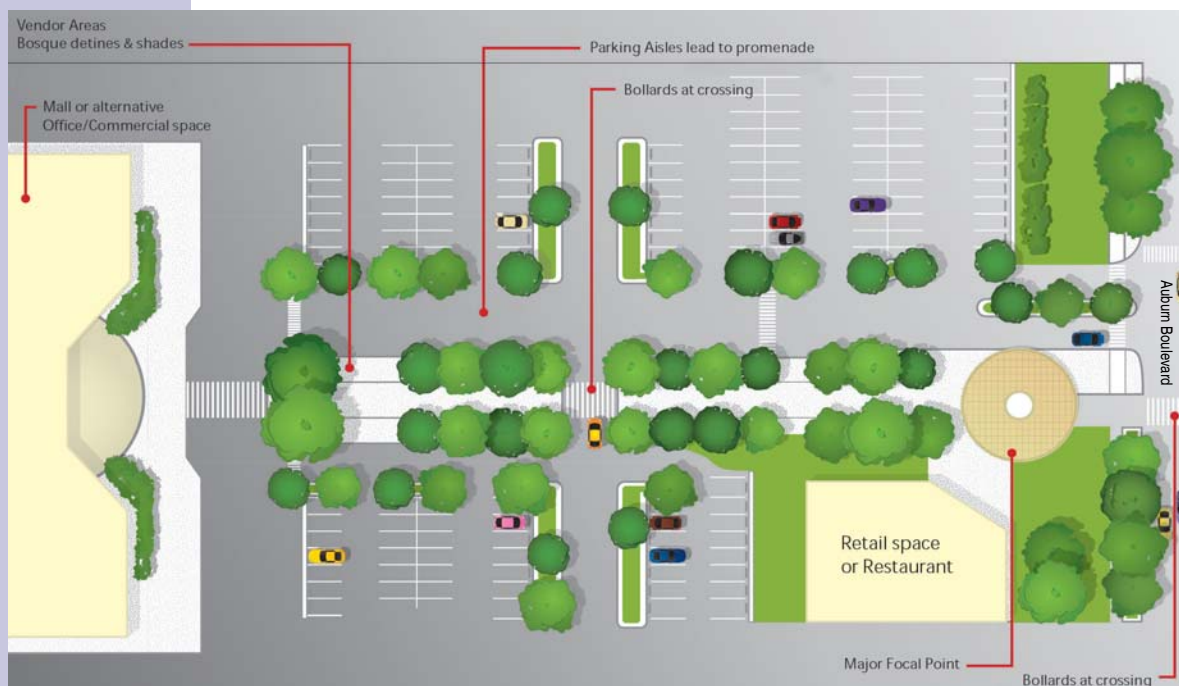
F. Site Design and Building Orientation

- Structures should be sited in a manner that will compliment the adjacent structures. Sites should be developed in a coordinated manner to provide order and diversity to avoid a jumbled, confused development.
- Locate structures and on-site circulation systems to minimize pedestrian/vehicle conflicts. Enhance the sidewalk with textured paving, landscaping and trellises where feasible.

- Locate loading facilities at the rear of the building. Loading docks and doors must be screened from the street.
- Whenever possible, new structures should be clustered to create plazas or pedestrian malls and prevents long “barracks-like” rows of structures. A visual link between separate structures should be established through the use of an arcade system, trellis or other open structure.
- Cluster uses will include a well defined public space, such as a plaza or pedestrian mall. The common public area will provide an area not less than two percent of the total leasable floor area of the site and will be designed to accommodate tables and benches for outdoor dining and casual seating, as well as carts for outdoor vending. Such areas may be allocated to a specific use, such as a restaurant.
- Each retail center will provide a primary walkway linking all stores and shops. The primary walkway will have a minimum width of eight feet adjacent to retail and five feet in parking areas and pedestrian scale lighting to promote evening use.
- A tree canopy should be provided over the parking area, but allow the primary entry to be seen from the street. The County’s Shade Tree Ordinance will be adhered to in all cases.

G. Pedestrian Paths within Parking Areas

- Building entry zones will be clearly defined through the use or combined use of elements such as accent paving, planting, color pots and bollards.
- Multi-family residential uses in a mixed use project will have a building entry separate from the non-residential uses.
- Enhanced paving, striping or other distinguishing design feature should be used selectively on the primary walkway and the common areas to emphasize special areas and to guide pedestrians.



- Separate vehicular and pedestrian circulation systems should be provided. A clearly defined five foot pedestrian way separated from vehicle traffic will be provided through the parking area to a primary building from the sidewalks along the adjacent streets.
- Within parking areas the pedestrian path should be parallel to moving cars to minimize the need for pedestrians to cross parking aisles and landscape areas.
- Bicycle racks or lockers should be located in highly visible and convenient areas, but will not obstruct the pedestrian walkways.
- Site layouts will be designed to provide pedestrian access from the off-site pedestrian circulation systems to residential units and common area facilities that minimize conflicts with vehicles to the greatest extent possible.

H. Public Transit Access

- Commercial developments adjacent to a public transit route will provide a paved direct sidewalk from the primary facade of the major building to the sidewalk along the Boulevard leading to a transit stop. Where multiple buildings are included in the development each building with a gross floor area over 50,000-square-feet will provide a connecting path that permits occupants to easily walk to the nearest transit stop.

I. Security

- Defensible space design techniques will be incorporated in all areas. Dense landscaping near structures and on the periphery of parking areas will be limited in order to maintain view corridors.
- Locate potential crime risk uses, such as automatic teller machines in highly visible and well-lighted areas.
- Maximize the visibility of parking area entrances from adjacent uses and public streets.

J. Internal Driveway Circulation

- At the main entry to building entries, short-term parking for delivery and service vehicles will be provided such that they will not block pedestrian and vehicle circulation routes.
- Parking areas will be designed to provide space for unloading delivery trucks and to allow delivery trucks to head rather than back on to the street.
- Loading and unloading delivery vehicles in the street or in the median will not be permitted on the Boulevard.
- Internal driveways that connect between parcels, particularly of deep and narrow shaped parcels, are encouraged to minimize the number of driveways on the Boulevard and provide improved access to the rear of these parcels.
- Where parking areas are connected, direction of travel and parking bays should be similar to reduce conflicts at points of connection.

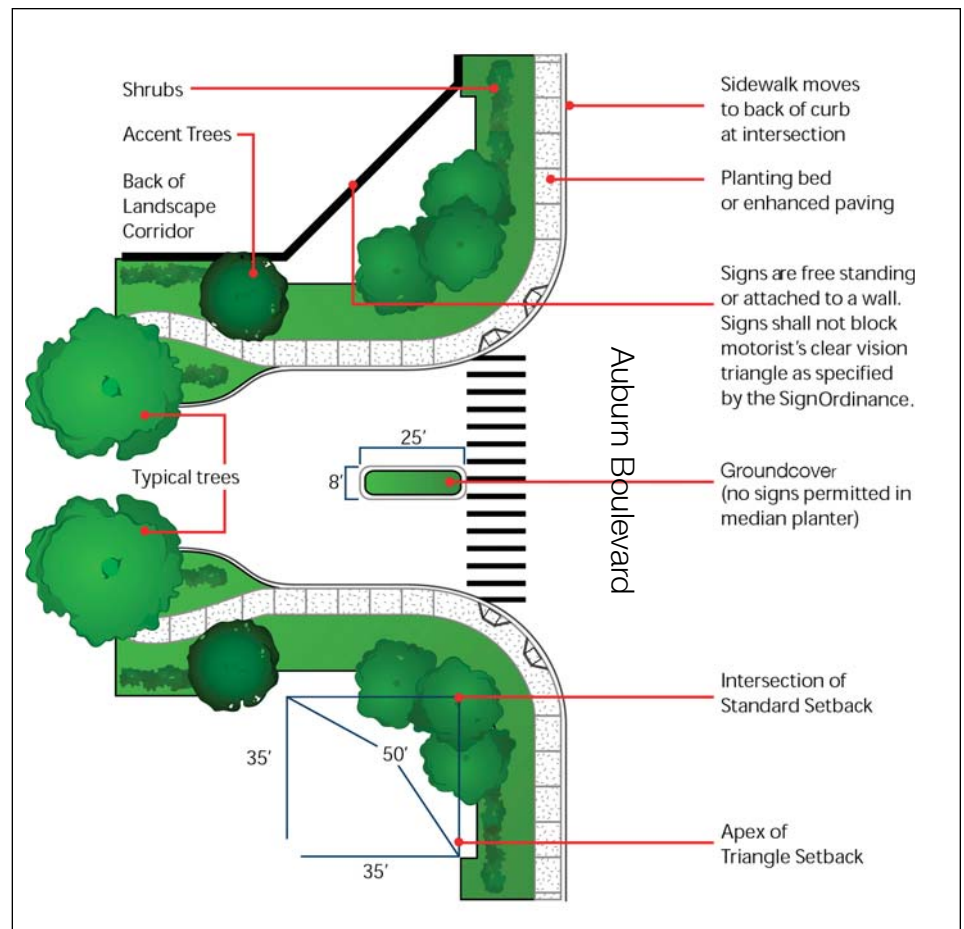
- On-site circulation should be designed to discourage speeding by avoiding long straight drives where conflicts with pedestrians and parked cars can occur. Speed bumps are strongly discouraged.
- Side or rear loaded delivery bays (and associated service/refuse areas) are preferred to front loaded delivery bays and should be screened from the pedestrian ways by a wall and/or landscaped berm.

Common Access Driveways

The number of relatively narrow properties along the Boulevard suggests the need to consolidate driveway entries to minimize the number of turning movements on the street.

- Shared access drives between adjacent parcels are encouraged to minimize curb cuts.
- Shared parking facilities between compatible adjacent uses are encouraged.
- Common driveways which provide vehicular access to more than one site are encouraged. The number of access driveways should be limited to the minimum amount necessary to provide adequate circulation.
- Shared parking between adjacent businesses and/or developments is encouraged whenever practical.

K. Parking Requirements



Proposed Typical Entry

On street parking will not be permitted on West Auburn Boulevard.

Parking standards, particularly the number of parking spaces required to accommodate a certain type of activity, are based on the activity rather than the type of building. Therefore, the form-based zone must rely on conventional parking requirements. However, the configuration of parking areas can relate to the form-based zone.

This section addresses the configuration of parking areas, rather than the number of parking spaces required. The majority of uses on the Boulevard will require conventional parking areas and on-site circulation (driveways and loading areas). Off-street parking will conform to “Chapter 30: Off-street Parking” in the County Zoning Ordinance to ensure that off-street parking spaces and loading spaces are sufficient in number to accommodate all vehicles which will be congregated on a property.

- Multi-family residential uses in a mixed use project will have a separate parking area from the non-residential use secured in a manner that precludes non-resident vehicles from entering.
- Parking areas will be provided in the front of the building for customers.
- Parking areas will provide sufficient space to park and unload transport vehicles. Transport unloading in the street is prohibited.

L. Landscaping and Site Improvements

Landscaping

The landscape will establish an overall image and visual order, provide shade in the summer; allow solar gain in the winter; buffer particular uses, and guide and direct views. Public walking areas, including plazas and courtyards between buildings typically will be covered with hard surface paving and the landscaping will be in distinct planting areas. Tree placement will be organized to define walkways and public spaces.

These standards are intended to provide a neat and well-maintained appearance in areas not covered by buildings or parking, to enhance the existing site character in order to minimize the adverse visual and environmental impacts of large paved areas and to conserve water.

- In locations away from building facades, the primary walkway will be lined with deciduous canopy trees or comparable shade structures to shade the walkways and gathering areas.
- Landscaping for commercial uses should define entrances to buildings and parking lots, provide transition between neighboring properties (buffering) and provide screening for loading and equipment areas.
- Landscaping should be in scale with adjacent structures and be of appropriate size at maturity to accomplish its intended purpose.
- Landscaping around the entire base of structures is recommended to soften the edge between the parking lot and the structure. This should be accented at entrances to provide focus.
- Deciduous shade trees will be planted to shade sidewalks. These trees will be placed at least 15' away from the face of any building wall.



- Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks or the use of curbs.
- The use of vines and climbing plants on buildings, trellises and perimeter garden walls is strongly encouraged.
- Plants in boxed, clay or wood containers should be used for enhancement of sidewalk shops, plazas and courtyards.
- At maturity, trees should provide a shade canopy for all parking areas (other than auto sale lots).
- Landscaping should not obstruct visibility at drive aisle intersections.
- Bollards may be placed to control vehicular traffic and pedestrian flow adjacent to the parking area.
- Enhanced paving, striping or other distinguishing design feature should be used selectively at building entry areas, on the primary walkway and the common areas to emphasize these areas and to guide pedestrians. Examples of enhanced materials include pavers, stamped concrete, stained or integral colored concrete, scored patterns, and special finishes such as exposed aggregate.

Walls and Fences

- If not required for a specific screening (i.e. security or separation of incompatible land uses) walls should not be utilized within commercial areas.
- When used, walls should be designed to blend with the site's architecture. Both sides of all side-yard walls or fences should be architecturally treated. Landscaping should be used in combination with all walls.
- Security fencing and long expanses of fence or wall surfaces should be architecturally designed to prevent monotony.

Site Lighting

Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping, receiving, pathways and working areas.

- Architectural lighting should provide functional lighting for the safety of pedestrian movement.
- The design of light fixtures and their structural support should be architecturally compatible with the main structures on site. Light fixtures should be integrated within the architectural design of the structures.
- All building entrances should be well lighted.
- All lighting fixtures must be shielded to confine light spread within the site boundaries.
- Site lighting will provide a safe pedestrian-friendly environment.
- Lighting in pedestrian areas will use a common light fixture compatible with the buildings and will not exceed 12' in height.
- Light standards for parking areas will not exceed 20' in height.
- Lights in parking lots will not be co-located with trees in planters. The lighting and landscape plan must be fully integrated and coordinated.

Refuse Collection, Utility Service Areas and Outdoor Storage

The utilitarian elements of the site plan, including outdoor storage, utility service areas, and refuse collection areas are to be screened from the public use areas and streets by a solid wall. The wall will be constructed of the same material and have same finish material and color as the primary building. The height of merchandise, materials, and equipment stored or displayed will not exceed the height of the screening fence or wall.

- Any outdoor equipment whether on a roof, side of a structure or on the ground, must be appropriately screened from view. The method of screening must be architecturally integrated with the adjacent structure in terms of materials, color, shape and size. Where individual equipment is provided a continuous screen is desirable.
- All refuse storage areas will be self-contained to prevent spillage or leaching of liquids or other materials into underlying soils and designed to contain all refuse deposited between collections.
- Planters around trash enclosures are strongly encouraged. The use of shrubs and vines on enclosure walls provides an opportunity for additional green space as well as mitigates for graffiti on enclosure walls.
- Auxiliary buildings, such as kiosks, maintenance sheds or maintenance buildings visible to customers shall complement and integrate the design characteristics applicable to the larger buildings.

Storm Water Management / Water Quality Enhancements

Water quality features that settle and filter run-off from streets and parking lots will be incorporated in project site design.

- The water quality features should be treated as a landscape design feature.
- Grassy swales and other best management practices are encouraged to filter storm water.

M. Signs and Graphics

Outdoor Graphics

Public art in this SPAO can include murals, fountains, and sculpture of various types. The intent is to provide attractive visual elements that relate to the commercial uses in the area and the character of the community.

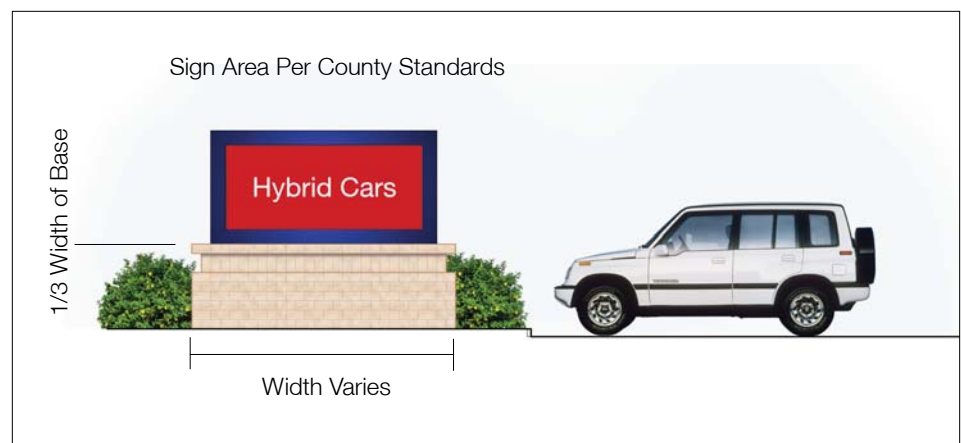
- Sculpture should be scaled to be clearly visible and notable from the freeway.
- Kinetic sculptures that move in response to wind, thermal changes or other natural forces are highly desirable. However, the motion of sculpture should not be frenetic or otherwise distract drivers.
- Art should not be clearly identifiable with a single manufacturer or product. However, art that relates to the "Inventors Row" design concept is encouraged.
- Public art in the form of murals and/or three dimensional relief sculptures may be attached to a building or screen wall. Art in these forms does not count against the allowable signage for that building or use.

Signs

The sign regulations are intended to eliminate excessive and confusing sign displays, preserve and enhance the appearance of the Plan area. The SPAO will include a variety of businesses with different advertising needs and different architectural styles. There is no need to establish a single design standard or theme that applies to all businesses, doing so could possibly conflict with the style of the individual buildings. The SPAO encourages distinctive and creative signage that is integrated and harmonious with the buildings and site.

Common Sign Program for the Streetscape

The sign program provided in the Streetscape Master Plan is the common signage element that will visually connect all properties and businesses in the SPAO. This Plan includes a signature blade sign that will identify



Proposed Typical Sign

the SPAO as a distinctive district. The signature signs will be located at intervals specified in the Streetscape Master Plan at the edge of the public right-of-way on the south side of the street. The County will have the right to acquire the land for these improvements and expand the public right-of-way.

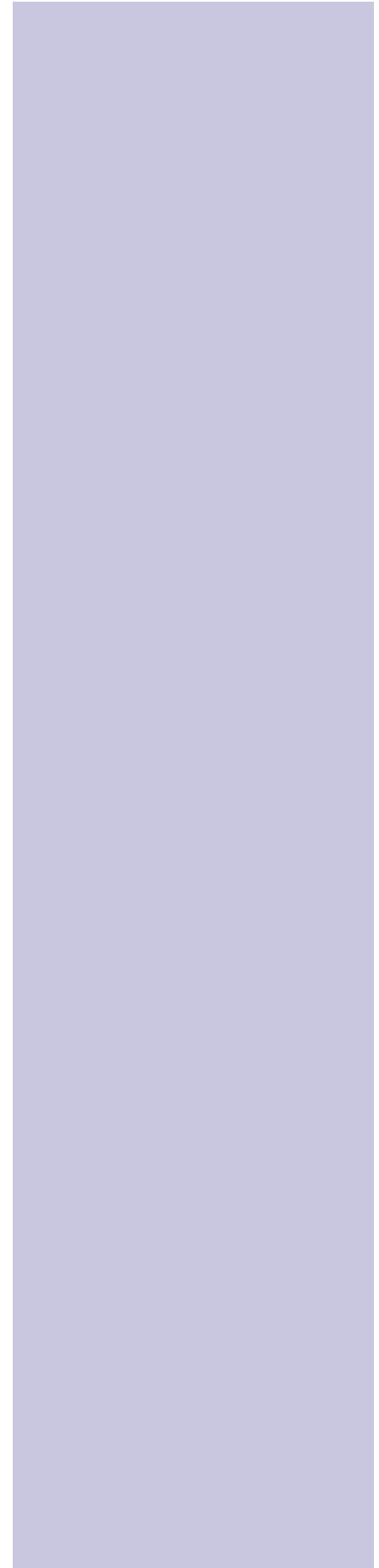
Signs within the West Auburn Boulevard SPAO will be regulated by the Sacramento County Zoning Ordinance Chapter 35: Sign Regulations except as specifically provided in this section.

The SPAO is within the Arden-Arcade community Special Sign District and is subject to the provisions of Article 5: Special Sign District except as provided in this SPAO.

The SPAO is also within Interstate-Business Route 80 Special Sign Corridor. However, it is not subject to the provisions of Article 3: Special Sign Corridors. The Special Sign District regulations of Article 5 prevail.

- Each commercial site developed as an Auto, Large Box, Cluster or Residential and Residential/Mixed Use Building type will have a Master Signage Plan to ensure quality and continuity of appearance.

- Monument-type signs are the preferred alternative for business identification.
- Each site should be appropriately signed to give directions to loading and receiving areas, visitor parking and other special areas.
- The wording of signs may describe the products sold provided that the sign is compatible with the character of the sign program and complies with all other standards. No sign makers' labels or other identification will be permitted. Signs will identify and locate rather than advertise and sell.
- Building signs will be integrated into building architecture and composed of materials compatible with the materials of the building.
- Banners, flags, pennants, shields and other graphic art that is part of a Master Signage Plan will be permitted subject to a design review. (Amends Section 335-09 [l and m], Prohibited Signs).
- Moving or rotating signs that rotate at less than 10 revolutions per minute or repeat a complete movement cycle at less than five repetitions per minute shall be permitted. (Amends Section 335-09 [a and b], Prohibited Signs).
- Inflatable or lighter-than-air devices of any kind including: those depicting animals, characters or vehicles when attached or secured from the ground or to any object on the ground or to any tree, building, sign or other built surface are prohibited. (Amended 11/90) (Amends Section 335-09[h], Prohibited Signs).
- Painted window signs and printed signs that obscure more than 20 percent of the window area shall be prohibited.
- Electronic message signs are regulated per County Zoning Code.



SECTION FIVE: Streetscape Master Plan



Niello Audi Dealership - 2006



Fulton Avenue Landmark - 2003

Section Five provides specific recommendations for aesthetic and infrastructure improvements to West Auburn Boulevard. These improvements are intended to elevate the image of the Plan area and establish a framework for redevelopment into a signature business address and regional commercial destination.

A. Scope and Intent of the Project and this Document

In conjunction with the Special Planning Area Ordinance, the Streetscape Master Plan recommends specific improvements throughout the Plan area. This Master Plan details streetscape improvements that can be undertaken over time, in a phased approach, allowing for the natural course of economic improvement to guide subsequent implementation phases.

B. Design Approach

A broad and eclectic group of destination-based businesses either already exist along the Boulevard or are encouraged. The consensus among local business owners, the Technical Advisory Committee, stakeholders, designers and consultants is that the design approach should capitalize on the burgeoning economic growth and integrate the Boulevard's historic significance with contemporary commercial uses, which are currently dominated by auto dealerships and complementary businesses such as hotels. Highlighting the Boulevard's historic significance and current commercial uses in an interpretation that is modern, but not trendy, will transform the Boulevard into a viable commercial destination.

The design approach for the West Auburn Boulevard Streetscape Master Plan centers on six key elements:

- Unify the character of the Boulevard;
- Clean up the street right-of-way and adjacent freeway frontage;
- Enhance visibility to and from the freeway;
- Increase pedestrian accessibility and safety;
- Improve vehicular safety;
- Preserve West Auburn Boulevard's historic and commercial integrity.

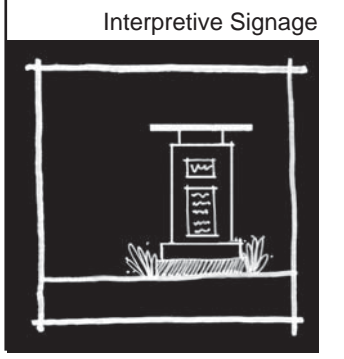
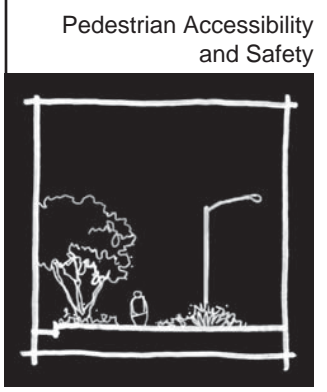
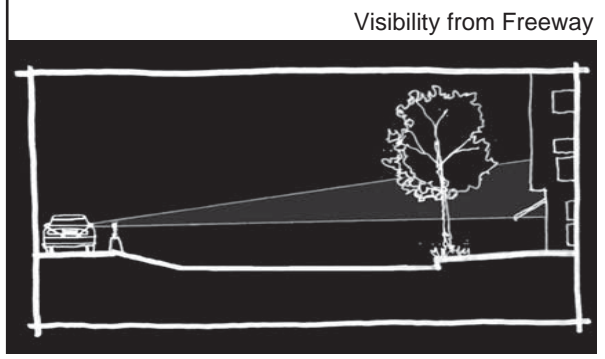
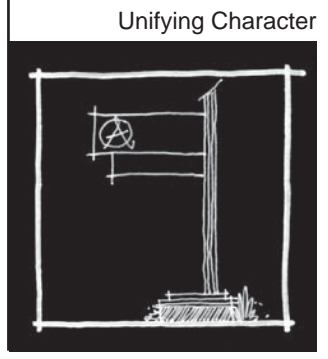
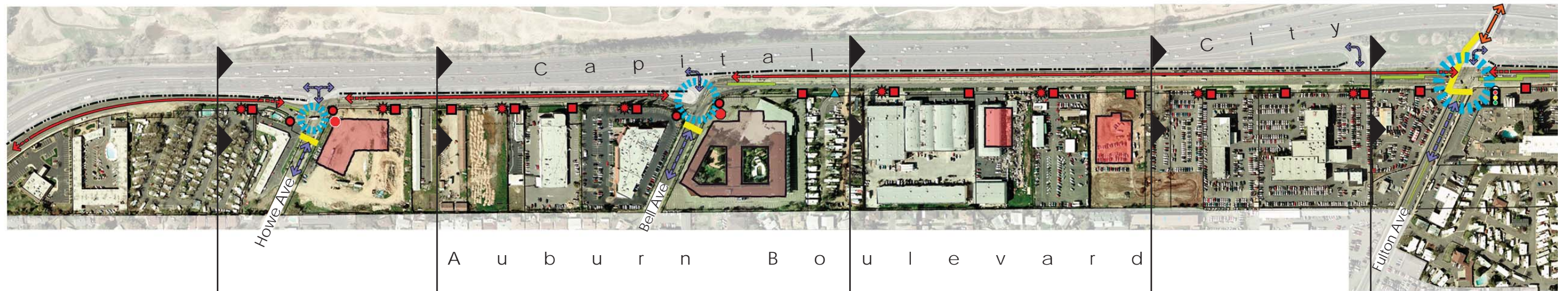
Unify the Character of the Boulevard

The Master Plan focuses on creating a unified street character by incorporating a consistent aesthetic that complements adjacent land uses and architectural character.

New signage will be used to create a unified aesthetic throughout the Plan area. These elements will be created using consistent materials that blend with existing streetscape areas and provide a framework for the hardscape palette.

The existing chain link fence that separates eastbound traffic on the freeway from the Boulevard will be replaced with a "K-rail" capped with

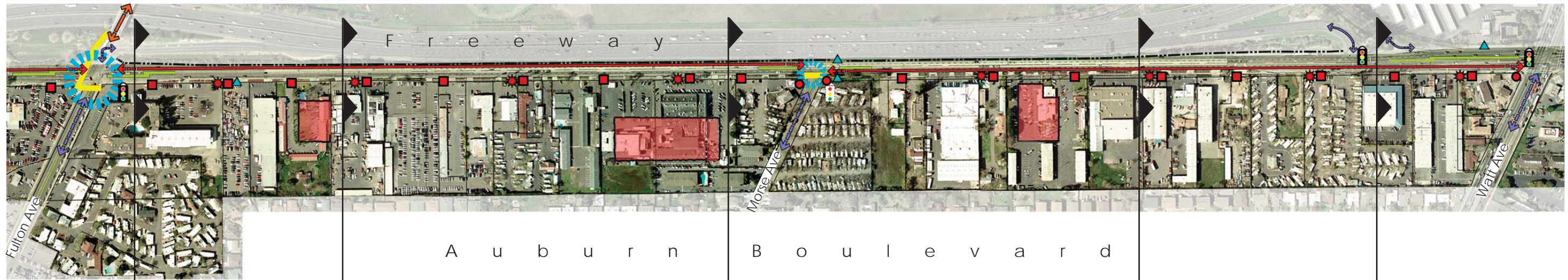
WEST AUBURN BOULEVARD - OVERALL MASTER PLAN



No Scale

Plan Legend

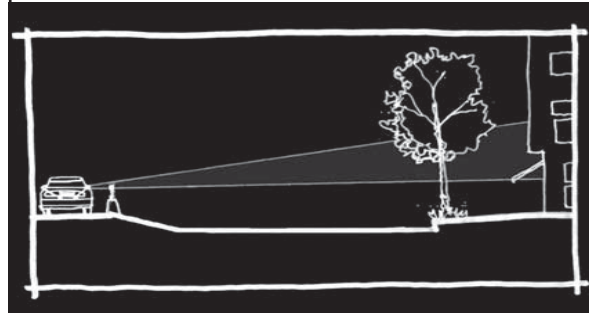
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| | Important/significant intersection | | Landmark Blade Signs |
| | Traffic flow along Auburn Boulevard | | Inventor Monuments |
| | Traffic flow along side streets | | Corner Monuments |
| | Freeway Overpass | | Pedestrian crossings |
| | Stop signs at intersections | | Bus stops |
| | On and off ramps to/from Capitol City Freeway and Auburn Boulevard | | Planted medians |
| | Signalized intersections | | K-rail and decorative fence |
| | Future Signalized intersections | | |



Pedestrian Accessibility and Safety



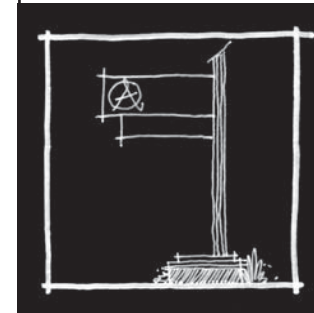
Visibility from Freeway



Vehicular Safety



Unifying Character


















Interpretive Signage



No Scale

Plan Legend

-  Important/significant intersection
-  Traffic flow along Auburn Boulevard
-  Traffic flow along side streets
-  Freeway Overpass
-  Stop signs at intersections
-  On and off ramps to/from Capitol City Freeway and Auburn Boulevard
-  Signalized intersections
-  Future Signalized intersections
-  Landmark Blade Signs
-  Inventor Monuments
-  Corner Monuments
-  Pedestrian crossings
-  Bus stops
-  Planted medians
-  K-rail and decorative fence

decorative fencing. The K-rail and fence may integrate public art, further enhancing the unique character and a consistent edge to the Plan area and enlivening the boundary between the Boulevard and the freeway.

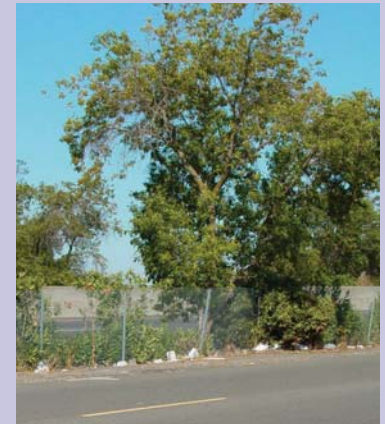
It is recognized that the street could accommodate landscape medians within the current right-of-way. However, property owners have expressed a serious concern with the inclusion of landscape medians within the project areas as it relates to accessibility to parcels and the potential impacts to current and future businesses. Prior to any consideration of the suitability of medians within the West Auburn Boulevard Project area, the County and SHRA will develop a process to engage the property owners and the public to consider the issue. Inclusion of landscape medians will only be considered upon full ensuring that each location does not obstruct parcel accessibility or cause undue harm to current or potential businesses.

Since pedestrian walkways run parallel to the street on its south side, the Master Plan recommends that south right-of-way frontages be improved to allow for six foot wide sidewalks, as required by the County of Sacramento’s Pedestrian Design Guidelines. Where required, existing sidewalks should be upgraded to maintain consistency along the entire street. Most of the Boulevard already includes a separated sidewalk condition. This sidewalk alignment is to remain and is encouraged, where viable, elsewhere along the Boulevard.

Key elements of the Boulevard—signage, plantings and plant palette and hardscape materials—should be designed to be modern, but not trendy. This could be construed as a European design aesthetic: clean lines and simple forms, focusing in depth on mass and scale rather than accent pieces or fussy details, but with classic materials and colors such as stainless steel and navy and red. The Niello Dealership constructed in 2006 sets the tone for the desired design aesthetic.

Clean Up the Street and Adjacent Freeway Frontage

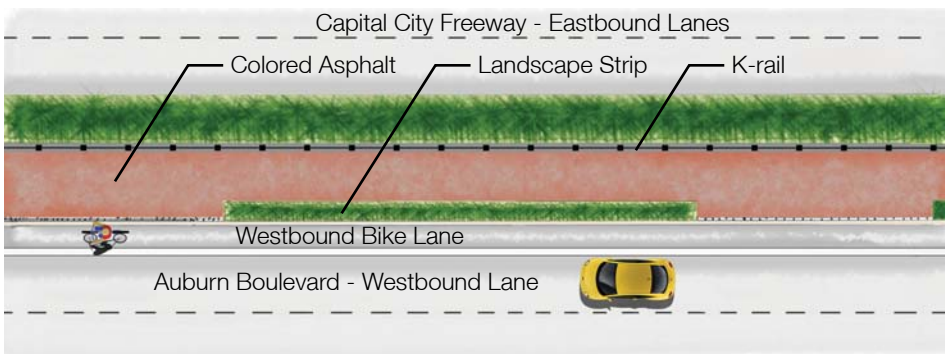
Maintenance within the Plan area is critically important to improve the quality of the street environment and to attract new businesses, as well as customers to existing businesses. West Auburn Boulevard has long suffered from inadequate maintenance, giving it an overlooked, forgotten appearance. A key component to making this Plan viable is to establish regular maintenance practices and code enforcement activities. A maintenance plan must be created with the adoption of the improvement plan to implement on-going maintenance practices.



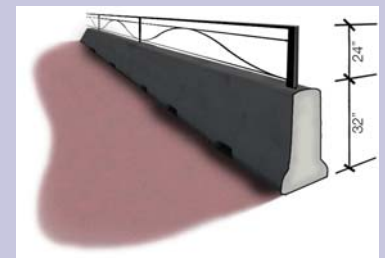
Poor maintenance plagues Auburn Boulevard



An unsightly Caltrans fence hinders views of the Boulevard from the Capital City Freeway



Typical K-Rail placement



Typical K-rail with decorative fence



Existing sidewalk locations hinders pedestrian safety



Poor crosswalk configurations limit pedestrian access to and from West Auburn Boulevard



Current off-ramps from the Capital City Freeway disrupt traffic flow along West Auburn Boulevard

Public realm improvements recommended in this Plan are geared toward ease of maintenance into the future. For example, the Plan includes installation of curb and gutter along the north side of the street to allow for street sweeping on a regular basis. In addition, the proposed “K-rail” and adjacent decorative paving will be easy to maintain. All recommended improvements must have low life cycle costs.

Enhance Visibility To and From the Freeway

Capital City Freeway, the stretch of I-80 adjacent to West Auburn Boulevard, is one of the most heavily traveled commuter corridors in the region. Improving visibility to West Auburn Boulevard from the freeway in a manner that attracts people to leave the freeway and patronize businesses along the commercial corridor is a key strategy supported by the Master Plan.

Stakeholders along the Boulevard have expressed their concern over how their businesses are currently seen from the freeway and how improvements to the streetscape could enhance or detract from this marketing opportunity. Plan area residents and business owners alike have stated their distaste for the unattractive chain link fence and haphazard landscaping that dominates the view of West Auburn Boulevard from the freeway. The Master Plan addresses these issues through a number of recommendations which are further detailed.

- Underground utilities as much as possible to improve the quality of the view to current and future businesses;
- Allow for a variety of alternative tree placements along the street frontage and in medians to maximize visibility for each business;
- Replace the chain link fence along the north side of the street with a K-rail and decorative iron fencing;
- Provide decorative paving combined with limited plantings between the K-rail and the street curb to address maintenance issues;
- Use high quality, enduring materials that contribute to a positive image throughout the Plan area and will improve the quality of the view along West Auburn Boulevard and from the freeway.

Increase Pedestrian Accessibility and Safety

While movement along West Auburn Boulevard consists primarily of vehicular traffic, the ability for pedestrians to travel safely and easily along the Boulevard and across the street is an important issue that was raised by all stakeholders. Pedestrian access from the surrounding neighborhoods to businesses on the Boulevard was recognized as equally important. To facilitate accessibility, walkways have been extended along the entire length of the street and down intersecting cross streets within the Plan area. Access for persons of all abilities will be provided by conforming walkways, ramps and landings to ADA and Title 24 guidelines.

- The County’s Bikeway Master Plan calls for bike lanes in both directions of Auburn Boulevard. County standards require that bike lanes be provided at a minimum width of five feet. This standard shall be implemented on both sides of the Boulevard. This standard shall be implemented on both sides of the Boulevard; however, the bike land width shall be widened to nine feet at bus stop locations.
- Four bus stops currently exist in the Plan area. Three stops are on the south side and one is on the north side of the Boulevard.

The stop on the north side is currently located mid-block and has no accessible route of travel to it. This stop, and the reciprocal stop on the south side, will require relocation to the Morse Avenue intersection to ensure accessibility. Shelters should be added to all bus stops.

- Existing traffic signals may require upgrading and the Morse Avenue intersection will require a new traffic signal due to increased pedestrian activity.
- Street lighting currently exists along much of the Boulevard, but there are a number of areas where it does not meet current improvement standards. Existing lights require maintenance, replacement and/or improvement. This Plan requires that existing streetlights be upgraded to meet current standards and that new streetlights be installed where needed to provide adequate and consistent lighting along the entire street. Streetlights and traffic signals will also be painted to provide a consistent color theme along the Boulevard.

Improve Vehicular Safety

Motorist safety and ease of mobility are paramount when planning for improvements to the West Auburn Boulevard streetscape. Businesses rely on easy access for their customers as well as for employees and vendors. Buses need the room and opportunity to pick up and drop off passengers at convenient locations. Retail businesses require adequate space for large trucks to maneuver and off-load their hauls. To improve vehicular safety and increase access to businesses, this Master Plan proposes to reorganize traffic circulation patterns along the Boulevard.

The current street configuration consists of lanes that are much wider than current standards and projected future uses require. Lanes are generally 15' to 17' wide and, depending on the location, two to four lanes with a center turn lane. Curb and gutter exists along much of the south side of the Boulevard. The north side of the Boulevard generally includes a gravel shoulder and a sparsely planted area before being separated from the freeway by a chain link fence.

The Master Plan recommends that the street section be reconfigured to decrease speeding and accommodate future vehicle needs, as well as the proposed bike lanes, medians and planting areas. Vehicular lanes will be restriped to 12' in width and accommodate an 11' middle turn lane/median. While it will be necessary to maintain multiple lanes along the Boulevard as it nears the Watt Avenue intersection, it is recommended that, where possible, the street be reduced to two vehicular lanes, a center turn lands and two bike lanes. Prior to implementation of new capital projects that would reduce the number of lanes along Auburn Boulevard a traffic study justifying the new lane configuration shall be submitted to the Sacramento County Department of Transportation for review and approval. The traffic study should also address any locations where the ability for U-turns will be impacted.

Business owners voiced concern that suppliers with large vehicles may not have enough room to turn and maneuver if medians are placed in the center of the street and the number of lanes is reduced to one in each direction. The SPAO guidelines encourage the aggregation of smaller parcels along the Boulevard, as well as combining driveways from adjacent businesses into one common driveway. By eventually reducing the number of driveways, medians can be placed more frequently without



Power lines detract from the Boulevard's appearance



Bell



Edison



Morse

Great inventors are honored in the names of streets that intersect Auburn Boulevard



The Mills Station in Rancho Cordova is an example of how an historic structure can be preserved and reused to make a positive contribution to the character of Auburn Boulevard



Intersection of Morse Avenue and Auburn Boulevard



Street lighting will provide a safer atmosphere along Auburn Boulevard and create a unifying element

affecting ingress and egress to businesses. The Streetscape Master Plan currently defines only those areas where medians can be placed immediately. Where these raised medians are shown, the bike lane on the south side of the Boulevard will allow for greater turn movement of large vehicles and passing through vehicles.

Preserve West Auburn Boulevard's Historic and Commercial Integrity

West Auburn Boulevard was part of Highway 40 or the historic Lincoln Highway, the nation's first transcontinental road. Until the addition of the adjacent I-80 in the early 1960's, Highway 40 served as the primary connection between San Francisco and Lake Tahoe and was lined with many travel-oriented businesses. Several hotels and a few historic structures built during this period can still be found along the Boulevard.

The purpose of the Boulevard has changed since its inception as a throughway. Over time, the commercial corridor has emerged as a regional destination for lodging, purchasing a vehicle or unique items such as musical instruments and furniture. During the public planning process for this Master Plan, stakeholder groups concluded that this niche destination retail district could serve as the impetus for revitalizing the Boulevard.

Preserving the Boulevard's historic value and unique features is an important aspect of the recommendations for improvements made in the Streetscape Master Plan and SPAO. The SPAO offers guidelines for improving private property that might contain these unique characteristics. The Master Plan builds on the SPAO by recommending that West Auburn Boulevard continues to be treated as a major transportation corridor and a destination for the region.

A unique feature within the Plan area is that major streets that intersect West Auburn Boulevard are named after prominent inventors. This feature can contribute to establishing a distinctive sense of place by incorporating each inventor's background and contributions into interpretative signage and monumentation at each intersection.

The existing street character changes dramatically surrounding the intersection with Morse Avenue. In this area, several dilapidated structures exist, one of which is a vestige of West Auburn Boulevard's past as Historic Highway 40. The community encouraged the redevelopment of this corner to include a mixed-use structure that would be fashioned more closely to the corridor's historic character by including specific architectural features and site design. For example, a former gas station and wayside stop has been transformed into a convenience store and gas station with an adjacent restaurant and living quarters above. These structures could be rehabilitated in a fashion similar to Mills Station in Rancho Cordova.

C. Detailed Improvements / Recommendations

The Master Plan recommends that enhancements for the entire Boulevard be addressed first, followed by additional improvements to particular sections of the Boulevard.

Specific improvements recommended for West Auburn Boulevard are

divided into several categories:

- Infrastructure and utility improvements;
- Vehicular, bicycle, pedestrian circulation and transit improvements;
- Aesthetic improvements.

Infrastructure and Utility Improvements

- Underground all overhead utilities running along the south side of West Auburn Boulevard between Janice Street and Watt Avenues and, where applicable, when running perpendicular to the Boulevard along property lines or through the centers of parcels.
- Provide street lighting throughout the Plan area to ensure the County's minimum standard of service. Painted cobra head streetlights are recommended. The use of painted cobra heads will lead to additional long term maintenance costs for the Sacramento County Department of Transportation. The creation of a Landscape and Lighting District as outlined in Section Six will help ensure future maintenance of the painted lights.

Vehicular, Bicycle, Pedestrian Circulation and Transit Improvements

Freeway Frontage

- Replace the existing chain link fence between the Capital City Freeway and eastbound traffic lane of West Auburn Boulevard with a "K-rail" capped with decorative fencing. The K-rail fencing would reach a total height of just under five feet; one foot lower than the current chain link fence. The decorative fence will be designed and maintained to appear relatively transparent, which will enhance the visibility to West Auburn Boulevard from the freeway. K-rail and fence can be used as a public art feature, provided that all Caltrans requirements are met.
- Decorative paving to match the decorative asphalt imprinted in the center turn lanes will be used in the width between curb and K-rail. In areas with greater than six feet in width, shrub planters material will be placed at regular intervals, separated by decorative paving. Additionally, a swath of paving will be provided between planting and K-rail to allow for easy removal of debris. The use of decorative paving the the center turn lane could lead to additional long term maintenance costs. In locations where utility companies need to trench beneath the pavement they are not required to replace decorative pavement in-kind. In future trench repair sections this will eventually result in an unsightly combination of decorative pavement and conventional asphalt concrete. It will be necessary to spend future maintenance monies to prevent this undesirable outcome. The creation of a landscape and lighting district as outlined in Section Five will help ensure the future maintenance of the decorative pavement.
- Provide four foot minimum width planting areas spaced equally, and/or decorative paving between K-rail and West Auburn Boulevard.



Approved decorative fencing and K-rail can enhance the Boulevard and contribute to traffic safety



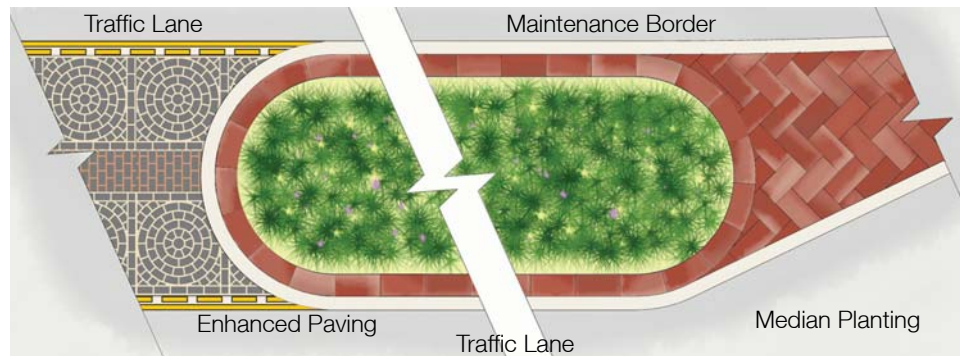
Duratherm decorative pavement will be used as an at-grade median where necessary, further unifying the character of West Auburn Boulevard



Bus stop shelters are oriented to face adjacent properties instead of the street

Median/Center Turn Lane

- Provide an 11' enhanced center turn lane throughout the Plan area through the use of custom decorative imprinted asphalt.
- Where space and driveway access allows, provide a raised and planted median.



Typical median layout

- Medians will consist of plant materials surrounded by a colored concrete maintenance border and separated from asphalt by a barrier curb, per Sacramento County improvement standards. All planted medians will require root barrier protection.
- Median noses and areas too narrow for plantings will be paved with concrete pavers.
- Center turn lane delineation markings will be provided by solid fill in the border of the decorative imprinted asphalt and reflective dots.
- Potential median locations will be analyzed and ultimately located in subsequent phases of design work (Streetscape Improvement Plans). Property owners along the Boulevard will be consulted in the decisions in where to place raised medians.

Surfacing and Striping

- Vehicular lanes will be restriped to 12' in width and accommodate an 11' center turn lane or strategically placed raised, planted median. While it will be necessary to retain a multiple lane configuration, it is recommended that, between Bell and Watt Avenue intersections, the street be reduced to two vehicular lanes (where possible), a center turn lane, and two bike lanes. Prior to the implementation of any capital projects that would reduce the number of lanes on Auburn Boulevard, a traffic study justifying the new lane configuration shall be submitted to the Sacramento County Department of Transportation for review and approval. The traffic study should also address any locations where the ability to U-turn will be impacted.
- Resurface street paving with rubberized asphalt throughout the Plan area.
- Reduce vehicle lanes to 12' width to allow for improvements.
- Provide curb and gutter along the north side of West Auburn Boulevard and in any locations within the Plan area that it does not currently exist.

Path of Travel Improvements

- All sidewalks along the Boulevard will be a minimum of six feet wide.

- Where space allows, sidewalks will be separated from the roadway by a planter strip of at least eight feet wide.
- Improve sidewalks to ensure uniform accessibility along entire street including curb, gutter, ramps and crosswalks.
- Provide signal improvements as required to ensure pedestrian improvements at all signalized intersections.



Typical bus stop layout

Bicycle Lanes

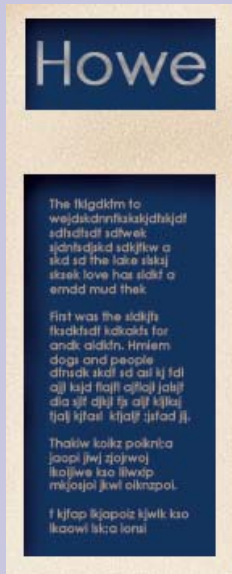
- Six-foot wide bike lanes will be the standard implemented on the north side of the Boulevard. However, on the south side, the bike lane will be widened to eight feet where space is available to accommodate right hand turn movements and bus stop locations.
- Add bike lanes in both eastbound and westbound directions. East direction will be eight feet wide; west direction will be six feet wide.



Typical street corner monument



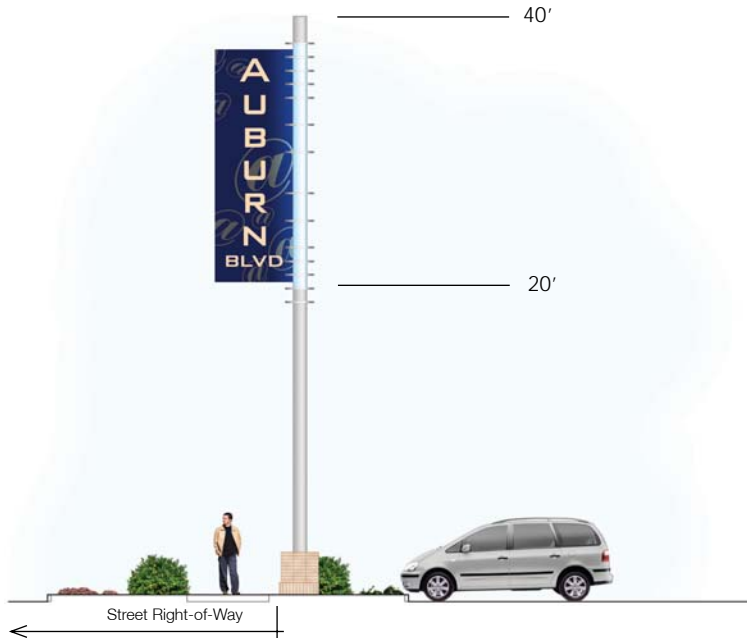
West Auburn Boulevard
Proposed Blade Sign



Proposed Interpretive Sign



Proposed Inventor Monuments



Blade Signs Visibility

Bus Routes / Stops

- All existing bus stops along the Boulevard will be enhanced with bus shelters, consistent benches, trash receptacles and bicycle racks.
- Bus stops will be oriented to face toward adjacent properties, with the back of the bus stop facing the Boulevard.
- The bus stops currently located at Deseret Industries will be relocated to the intersection at Morse Avenue to provide required accessibility across West Auburn Boulevard. Bus stop relocation should be concurrent with intersection signalization.

Street Amenities

- Add amenities to streetscape including benches, trash receptacles and bike racks.
- All existing bus stops along the Boulevard will be enhanced with bus shelters, consistent benches, trash receptacles and bicycle racks.
- Bus stops will be oriented to face toward adjacent properties, with the back of the bus stop facing the Boulevard.
- The bus stops currently located at Deseret Industries will be relocated to the intersection at Morse Avenue to provide required accessibility across Auburn Boulevard. Bus stop relocation should be concurrent with intersection signalization.
- Bike racks at a minimum of two locations within each block, equally spaced.
- Where limited tree planting space occurs, particularly in areas without separated sidewalk, provide street trees in tree grates.

Aesthetic Improvements

Signage / Monumentation

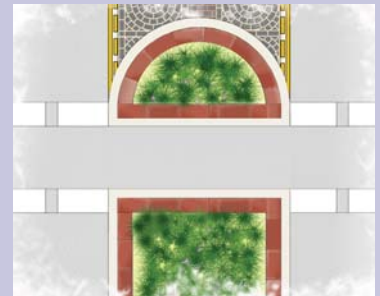
- Fifteen large scale Landmark Blade Signs will be placed along West Auburn Boulevard. Signs are to be placed equidistant between high tension power poles, but at the back of the sidewalk so the impact of the blade signs is not diminished by the power pole. Each sign will be 40' in overall height. The pole will be fiberglass and house blue tube lighting for dramatic impact at night. The sign will be constructed out of powder-coated sheet metal and mounted to the fiberglass pole in such a way as to mitigate wind and other environmental impacts. The pole will be painted to match stainless steel elements used on other areas of the Plan. The pole base will be constructed of four foot wide brick and will also include a planted buffer surrounding three sides. This sign will require structural engineering and right of way acquisition.
- Seven monuments commemorating the inventors after whom the streets intersecting West Auburn Boulevard are named will be provided at appropriate intersections. Howe, Bell, Morse and Watt Avenue intersections shall all receive this monuments. Each monument will be 10' in height and have a four foot square brick base. The general sign housing will be stucco, the primary street name will be lit and the sign itself created with a banked enamel/porcelain finish. The top piece will be fabricated from stainless steel. This monument will require structural engineering, final graphic and signage design and right-of-way acquisition.
- Thirty-two pedestrian-level inventor monuments will be placed along West Auburn Boulevard adjacent to and equidistant between the Landmark Blade Signs. These monuments are intended to be placed within the planting strip between the curb and sidewalk (within the right-of-way) where available, but may require right-of-way acquisition where separated sidewalks are not feasible. These monuments will be no more than three feet in height and placed so that visibility from driveways is not impeded. Each monument will contain a portion of the story of the inventors. The monument will be constructed primarily of brick with a baked enamel/porcelain sign. This monument will require final graphic design and potential right-of-way acquisition.

Public Art

- Opportunities for public art will be incorporated into the K-rail along the north side of the street. Public art could take the form of interesting fence work above the K-rail and/or paint work or impressions on the K-rail itself. All artwork involving the K-rail or K-rail fence must be approved by Caltrans.

Landscape Treatments

- Street trees will be provided along the south side of the street in a separated planter strip, if available. These trees will be chosen from the approved street tree list, as included, and will be placed an average of 30' on center.
- Accommodations will be made for the wider placement of street trees in front of businesses to allow for visibility, however these trees will not exceed 50' on center. On properties where this is required, a larger grouping of trees will be mandated at property lines and/or driveways to reach the average spacing noted. Sight



Cross walls at median

visibility in compliance with County standards will be maintained in all applications.

- Street trees in medians will be provided at a maximum spacing of 30' on center.
- A low growing ground cover will be planted within the parkway strip on the south side of the street.
- All street tree locations will require continuous root barriers along the length of curb and/or sidewalks to deter cracking from tree roots and irrigation water damage.

D. Street Section Recommendations

West Auburn Boulevard – Capital City Freeway to Bell Avenue

- Re-stripe the Boulevard to allow for one 12' vehicular and one six foot bike lane in each direction.
- Provide a median along West Auburn Boulevard extending west from Howe Avenue to protect the turn pocket.
- Provide a median along Bell Avenue extending south from the intersection of West Auburn Boulevard to within 50' of the first driveway.
- Provide a pedestrian crossing on the south side of West Auburn Boulevard at Howe and Bell Avenues that provides an at-grade crossing with median “safe zones” for pedestrians.
- Provide a six foot wide attached sidewalk along the south side of West Auburn Boulevard.
- Provide four landmark blade signs, three corner monuments and eight inventor monuments along the south side of the Boulevard as described earlier.

West Auburn Boulevard – Bell Avenue to Fulton Avenue

- Re-stripe the Boulevard to allow for one 12' vehicular lane in each direction and an 11' center median/turn lane. Additionally, provide one 8' bike lane on the south side and one 6' lane along the north side of the street.
- Add medians where they will not interfere with intersecting traffic from private driveways per County requirements.
- Where medians are not possible, provide a center turn lane denoted with decorative imprinted asphalt.
- Provide a pedestrian crossing for north-south and east-west traffic at intersection of Fulton Avenue, linking the existing freeway overpass to the Boulevard.
- Add a detached sidewalk on the south side of the street, separated from the roadway by a planting area.
- Provide five landmark blade signs, one corner monument and ten inventor monuments along the south side of the Boulevard as described earlier.

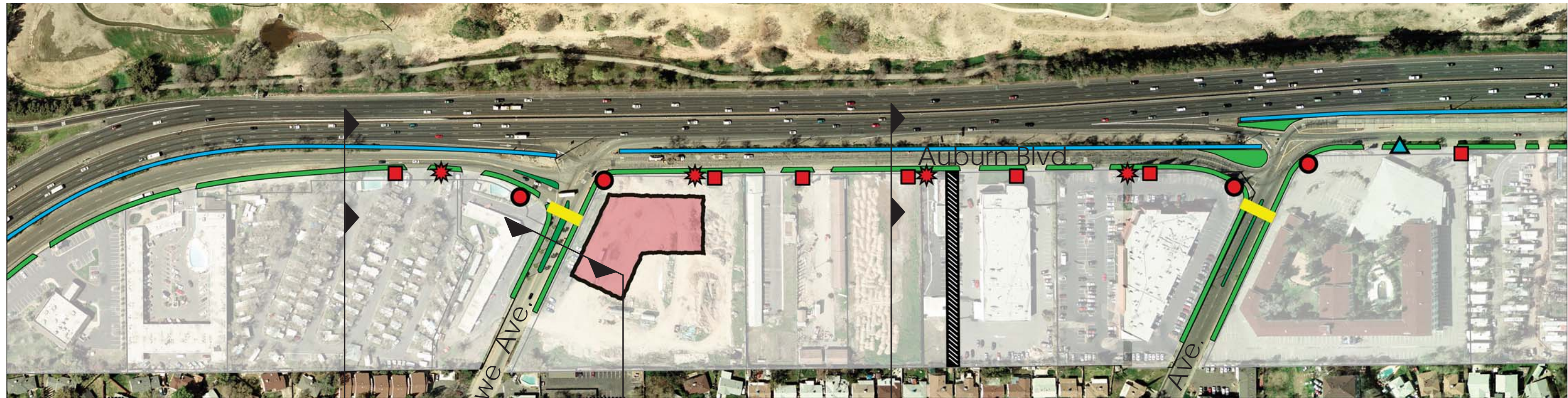
West Auburn Boulevard – Fulton Avenue to Morse Avenue

- Add a traffic signal at the intersection of Morse Avenue and West Auburn Boulevard.
- Re-stripe the Boulevard to allow for one 12' vehicular lane in each direction and an 11' center median/turn lane. Additionally, provide one 8' bike lane on the south side and one 6' lane along the north side of the street.

- Add medians along the street where they will not interfere with intersecting traffic from private driveways (as shown on plans).
- Where medians are not possible, provide a center turn lane denoted with decorative imprinted asphalt.
- Provide a median on Morse Avenue extending south to within 50' of the first driveway.
- Add a pedestrian crossing to West Auburn Boulevard at the intersection of Morse Avenue.
- Relocate the westbound and eastbound bus stops near Deseret Industries to just east of the intersection with Morse Avenue.
- Provide two bus shelters at the remaining existing locations.
- Add a detached sidewalk on the south side of the street, separated from the roadway by a planting area.
- Provide four landmark blade signs, one corner monument and nine inventor monuments along the south side of the Boulevard as described earlier.

West Auburn Boulevard – Morse Avenue to Watt Avenue

- Re-stripe the Boulevard to allow for one 12' vehicular lane in each direction and an 11' center median/turn lane. Additionally, provide one 8' bike lane on the south side and one 6' lane along the north side of the street.
- As West Auburn Boulevard approaches Watt Avenue, additional through and dedicated turn lanes are required to accommodate existing traffic conditions. The second westbound through lane will be re-striped to become a dedicated on-ramp onto the Capital City Freeway. As such, the number of lanes between Watt Avenue and the western on/off ramps will be unchanged.
- Add medians along the street where they will not interfere with intersecting traffic from private driveways..



See Section A, Page 5.19










See Section D, Page 5.21

See Section B, Page 5.19



No Scale

Plan Legend

-  Landmark Blade Sign
-  Corner Monumentation
-  Inventor Monumentation
-  Pedestrian Crossing
-  Transit Stop
-  Proposed Planting Areas/Medians
-  K-rail with Decorative Iron Fencing
-  Undergrounded Utilities
-  Proposed Building

Please Note

The County's Bikeway Master Plan calls for bike lanes in both directions on Auburn Boulevard. County standards require that bike lanes be provided at a minimum width of 5'. This standard shall be implemented on both sides of the Boulevard; however, the bike lane width shall be widened to 9' at bus stop locations.



No Scale

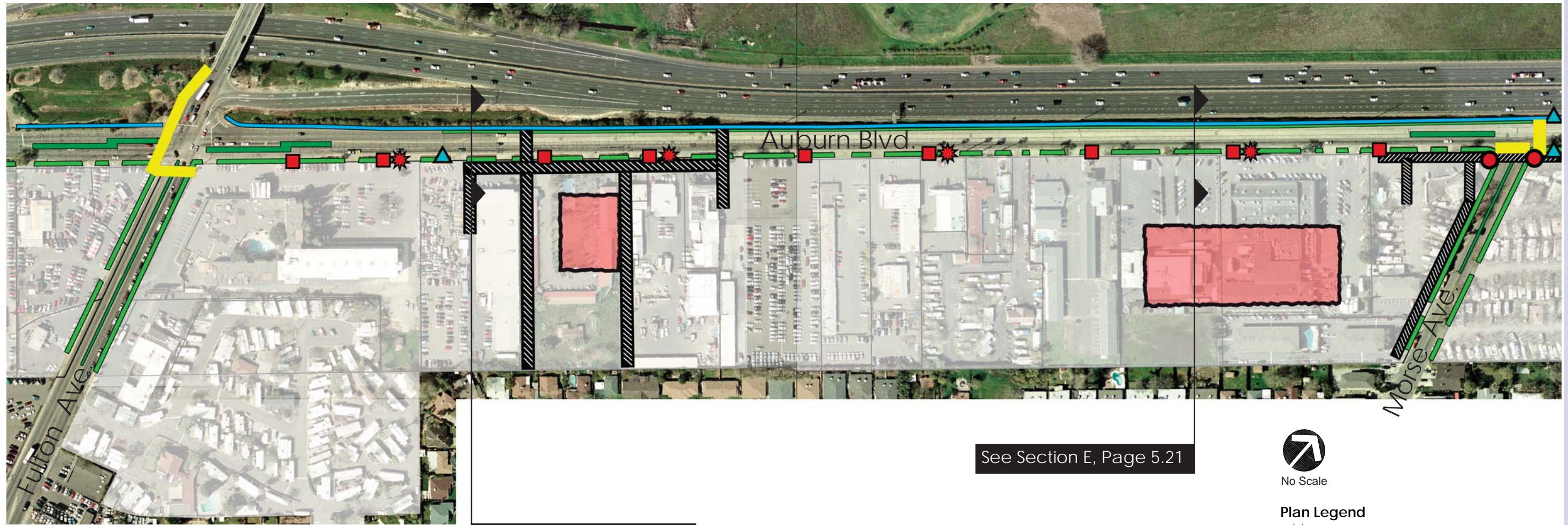
Plan Legend

-  Landmark Blade Sign
-  Corner Monumentation
-  Inventor Monumentation
-  Pedestrian Crossing
-  Transit Stop
-  Proposed Planting Areas/Medians
-  K-rail with Decorative Iron Fencing
-  Undergrounded Utilities
-  Proposed Building

See Section C, Page 5.20

Please Note

The County's Bikeway Master Plan calls for bike lanes in both directions on Auburn Boulevard. County standards require that bike lanes be provided at a minimum width of 5'. This standard shall be implemented on both sides of the Boulevard; however, the bike lane width shall be widened to 9' at bus stop locations.










See Section C, Page 5.20

See Section E, Page 5.21



No Scale

Plan Legend

-  Landmark Blade Sign
-  Corner Monumentation
-  Inventor Monumentation
-  Pedestrian Crossing
-  Transit Stop
-  Proposed Planting Areas/Medians
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-  Undergrounded Utilities
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Please Note

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No Scale

Plan Legend

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-  Corner Monumentation
-  Inventor Monumentation
-  Pedestrian Crossing
-  Transit Stop
-  Proposed Planting Areas/Medians
-  K-rail with Decorative Iron Fencing
-  Undergrounded Utilities
-  Proposed Building

See Section D, Page 5.21

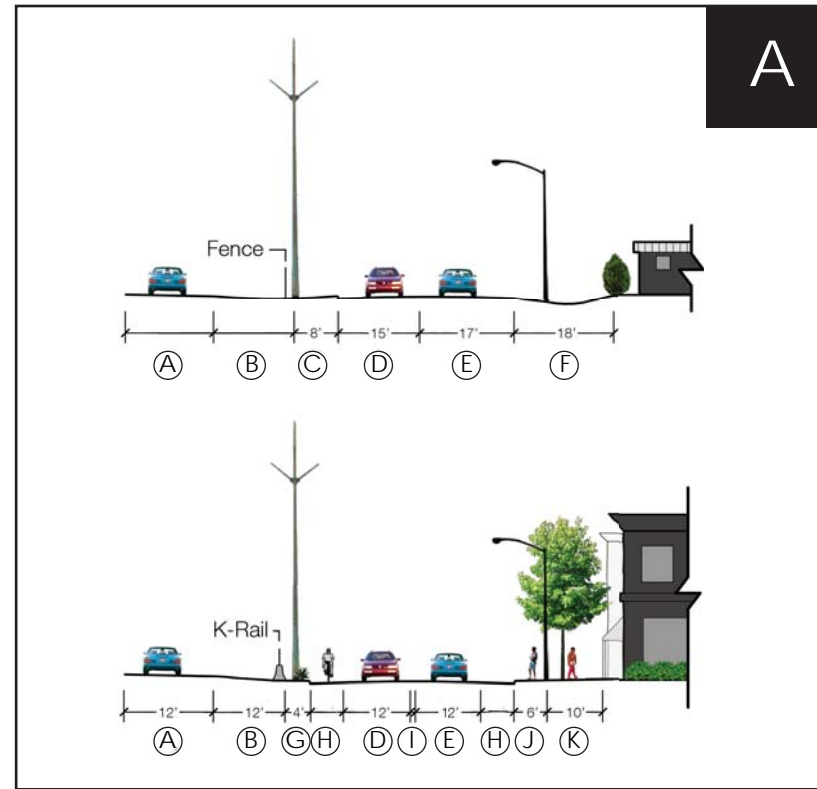
See Section E, Page 5.21

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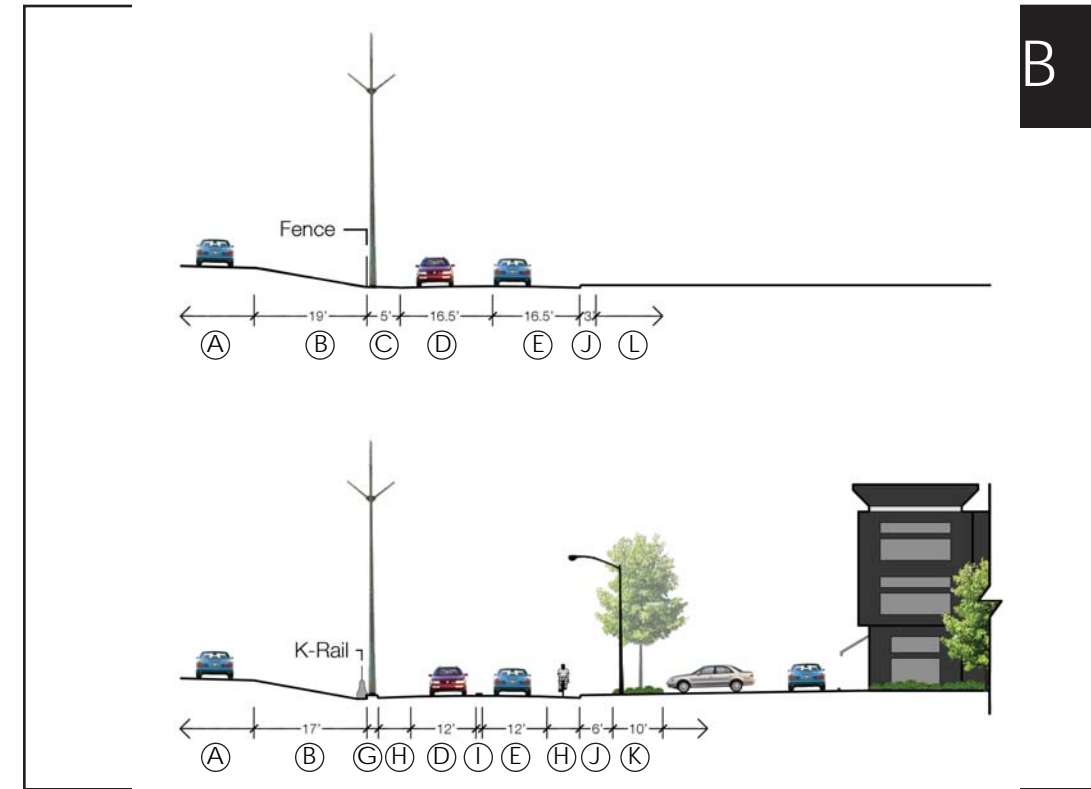
Legend

- A Eastbound Capital City Freeway
- B Highway Shoulder
- C Unimproved Roadside Shoulder
- D Westbound Lane
- E Eastbound Lane
- F Unimproved Street & Landscape Setback
- G Curb, Gutter, & Enhanced Paving/
Landscaping
- H 5' Wide Bike Lane (9' at Bus Stops)
- I Double Yellow Line Divider
- J Sidewalk
- K Minimum Landscape Setback
- L Vacant Parcel



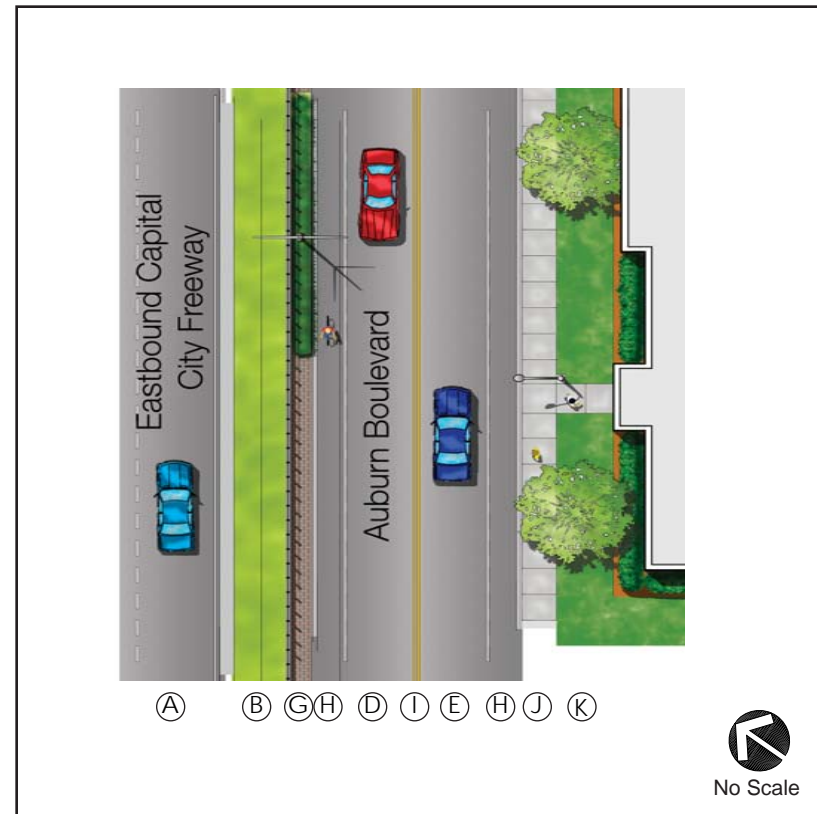
Existing Section

Proposed Section

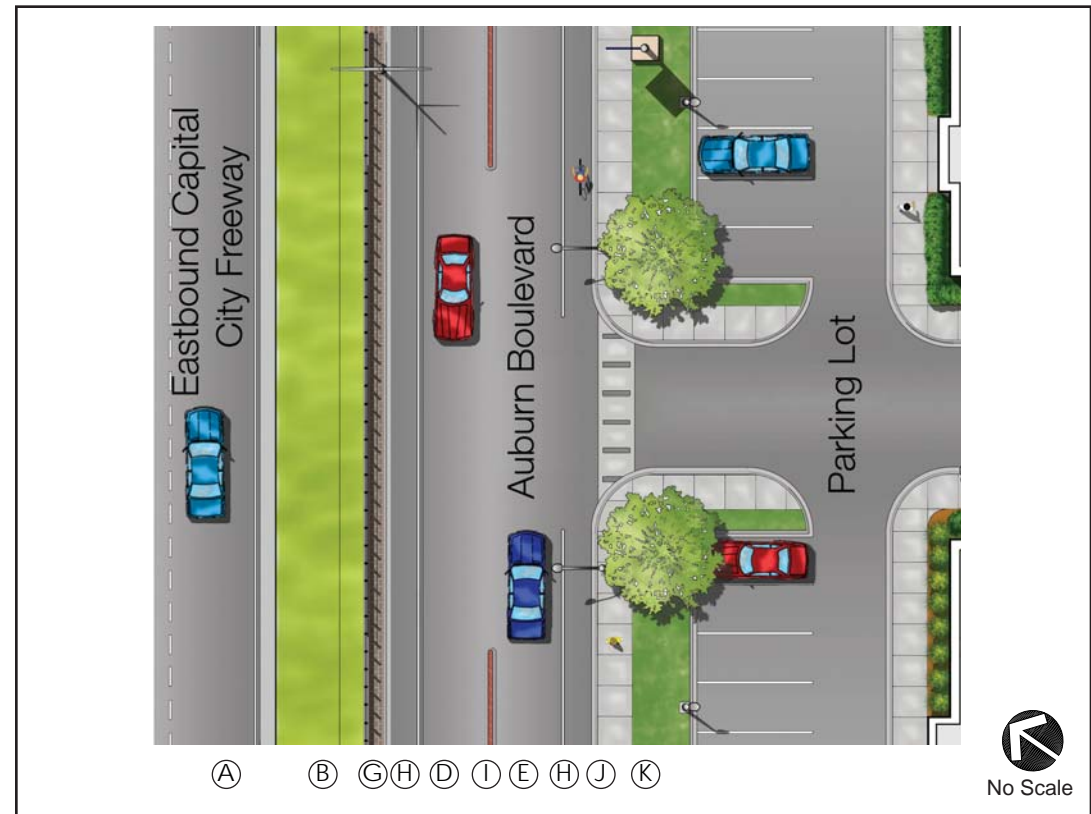


Existing Section

Proposed Section



No Scale



No Scale

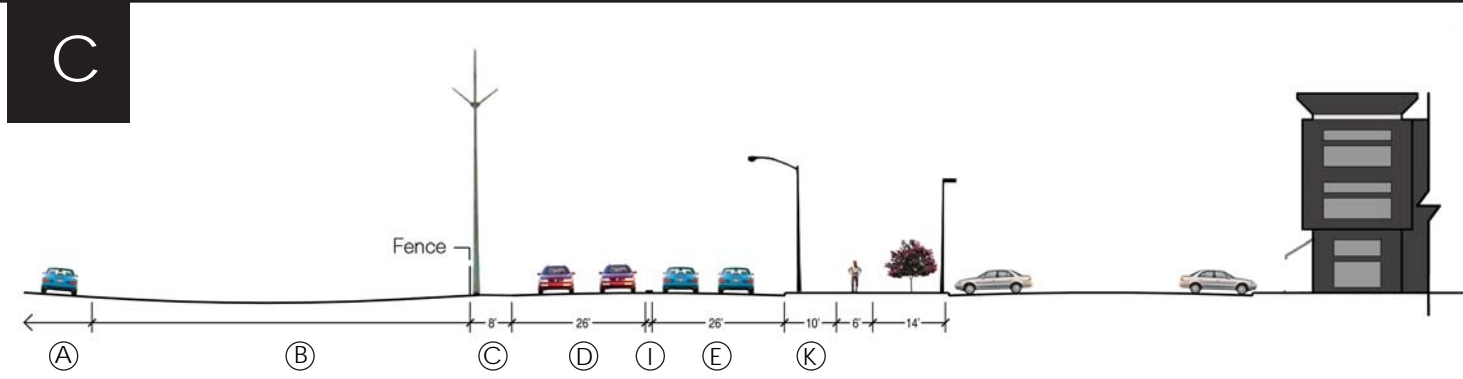
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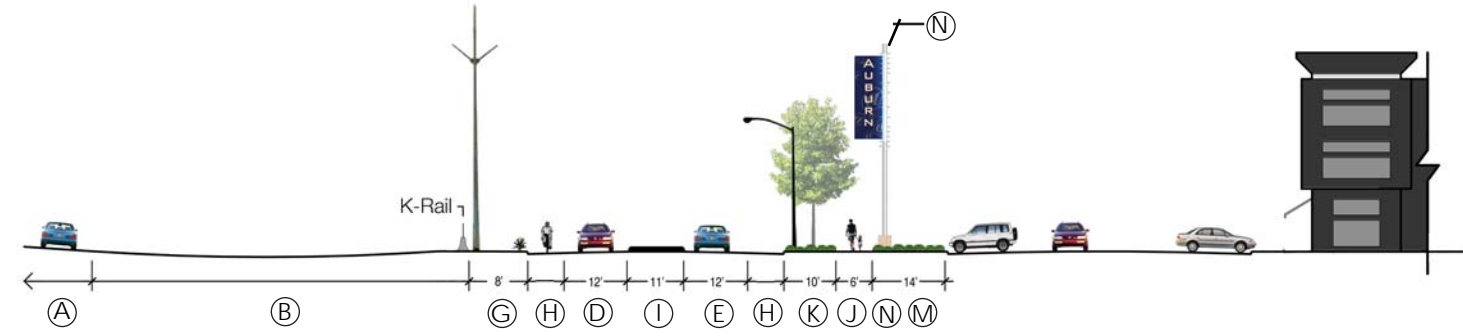
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- I Double Yellow Line Divider
- J Sidewalk
- K Parkway Strip
- L Vacant Parcel
- M Minimum Landscape Setback
- N Pylon Sign in Expanded Right-of-way

Existing Section



Proposed Section



Proposed Plan



A B G H D I E H K J N M



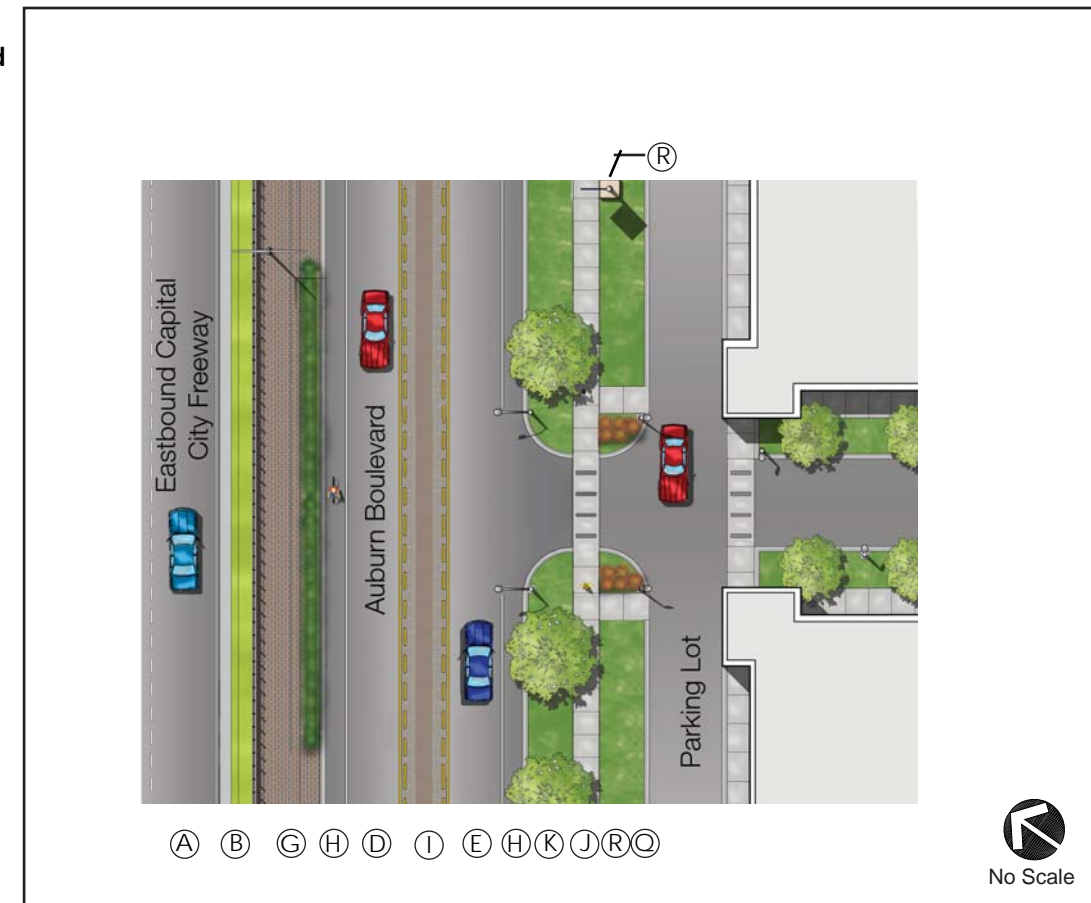
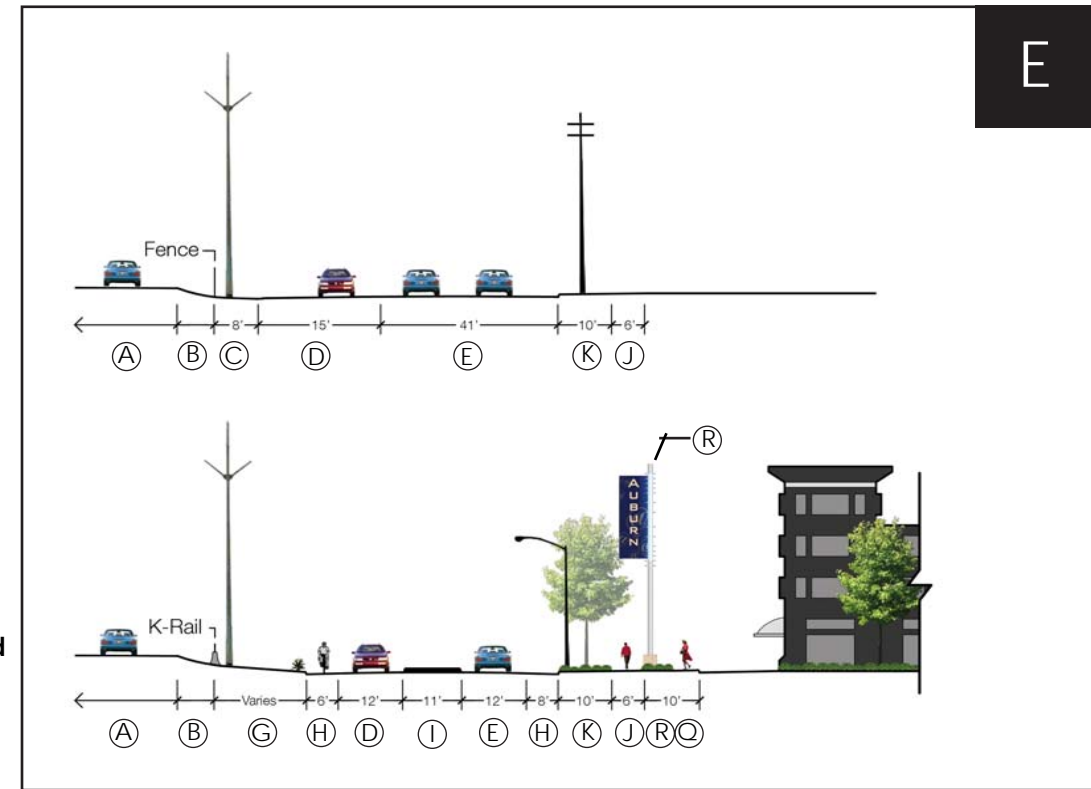
No Scale

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- L Vacant Parcel
- M Northbound Lane
- N Southbound Lane
- O Proposed Median
- P Curb and Gutter
- Q Minimum Landscape Setback
- R Pylon Sign in Expanded Right-of-way

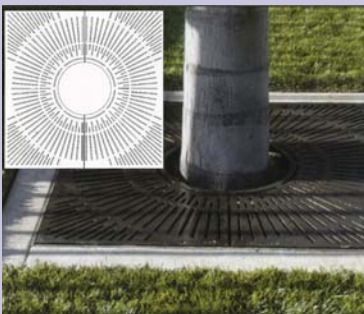


Please Note

The County's Bikeway Master Plan calls for bike lanes in both directions on Auburn Boulevard. County standards require that bike lanes be provided at a minimum width of 5'. This standard shall be implemented on both sides of the Boulevard; however, the bike lane width shall be widened to 9' at bus stop locations.



Materials Palette



- Where medians are not possible, provide a center turn lane denoted with decorative imprinted asphalt.
- In addition to bus stops at the intersection of Morse Avenue, provide a bus shelter at the existing stop location just west of Watt Avenue.
- Add a detached sidewalk on the south side of the street, separated from the roadway by planting area.
- Provide four landmark blade signs, two corner monument and eight inventor monuments along the south side of the Boulevard as described earlier.

E. Materials Palette

In keeping with the design approach described earlier, the Master Plan recommends a palette of furnishings and finishes that will complement the look of local businesses and create a uniform streetscape.

Furnishings

- **Bench**
 - Make: Landscape Forms
 - Model: Arcata Style, with aluminum slats
 - Color: Frame – Silver, Slats – Titanium
 - Comments: Embedded Mounting
- **Trash Receptacle**
 - Make: Landscape Forms
 - Model: Chase Park Style with side opening
 - Color: Titanium
- **Bike Rack**
 - Make: Landscape Forms
 - Model: Bola Style
 - Color: Stainless Steel
 - Comments: Embedded Mounting
- **Tree Grate**
 - Make: South Bay Foundry
 - Model: D11 Series
 - Color: Unfinished Weathered Cast Iron
- **Bus Shelter**
 - Make: Columbia Equipment
 - Model: Ridgford Park
 - Color: To match metal finishes shown below

Monumentation

- **Landmark Blade Sign:**
 - Base: Pacific Clay Brick – Summer Glacier
 - Pole: Fiberglass
 - Blade: Vinyl Coated Aluminum
 - Background Color: Sherwin Williams Loyal Blue (SW 6510)
 - Commercial “At” Symbol Color: Sherwin Williams Tradewind (SW 6218)
 - Lettering Color: Sherwin Williams Welcome White

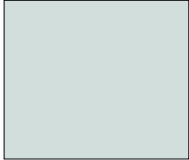


Bus stop shelters are oriented to face adjacent properties

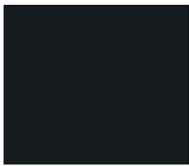
Finishes



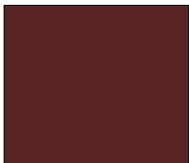
6510



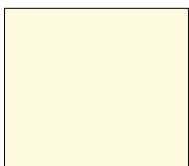
6218



6992



6328



6658

(SW 6658)

• **Corner Monuments**

- o Signage:
 - Manufacturer: Winsor Fireform
 - Color: Sherwin Williams Loyal Blue (SW 6510)
- o Monument:
 - Base: Pacific Clay Brick – Summer Glacier
 - Midsection: Semi Smooth Stucco – Color: Sherwin Williams Welcome White (SW 6658)
 - Top: Custom Aluminum

• **Inventor Monuments**

- o Signage:
 - Manufacturer: Winsor Fireform
 - Color: Sherwin Williams Loyal Blue (SW 6510)
- o Monument:
 - Base: Pacific Clay Brick – Summer Glacier

Lighting

• **Street Lighting:**

- o County Standard Cobra Head
- o Color: Painted - Sherwin Williams - Loyal Blue (SW 6510)

Finishes

• **Color Palette**

- o Sherwin Williams - Loyal Blue (SW 6510)
- o Sherwin Williams - Welcome White (SW 6658)
- o Sherwin Williams - Tradewind (SW 6218)
- o Sherwin Williams - Poinsettia (SW 6594)
- o Sherwin Williams - Inkwel (SW 6992)
- o Sherwin Williams - Arcade White (SW 7100)
- o Sherwin Williams - Fireweed (SW 6328)

• **Metal Finishes**

- o Landscape Forms “Titanium” (Metal-Polyester Powdercoat)
- o Landscape Forms “Silver” (Metal-Polyester Powdercoat)/ Stainless Steel
- o Unfinished Weathered Cast Iron

• **Brick, Stucco and Hardscape Finishes**

- o Imperfect-Smooth Stucco, Color: Sherwin Williams Welcome White (SW 6658)
- o Pacific Clay Brick: Western Ridge Summer Glacier
- o Concrete Pavers: Pavestone - Holland Stone, Sandstone Blend
- o Decorative Imprinted Asphalt: Duratherm Streetprint
 - Brick Border Color: Sherwin Williams Fireweed (SW 6328)
 - Customized Wagon Wheel: Sherwin Williams Arcade White (SW 7100)
 - Solid Fill: Brick “caps” per Duratherm streetprint in County Standard yellow striping color
- o Colored Concrete: LM Scofield - Barcelona Brown (1017)

Plant Materials

• **Trees**

- o Acer Rubrum- Red Maple
- o Washingtonia filifera-California Fan Palm

• **Shrubs**

- o Diets veaeta - Fortnight Lily
- o Indica- India
- o sima- Mex
- o x - New Z



Red Maple



California Fan Palm

may be accepted for use upon
 County of Sacramento.



New Zealand Flax



Fortnight Lily



Juniper



India Hawthorn



Star Jasmine



Mexican Feather Grass



Periwinkle



6594



7100



Titanium



Silver



Stucco



Streetprint



Summer Glacier



Sandstone Blend

SECTION SIX: Implementation Plan



Section Six evaluates the projected costs to implement the recommendations of the Streetscape Master Plan. An analysis of issues affecting these costs includes phasing considerations, regulatory policies, development standards, streetscape improvement policies, funding sources and administration of the Plan area.

A. Action Plan

In order to fully implement the Plan, several action steps must be followed:

- Certify a Environmental Document
- Adopt the Land Use Plan and Development Standards;
- Adopt the Streetscape Master Plan;
- Implement an Streamlined Review Program;
- Seek funding for public improvements and establish either an LLD or PBID;
- Conduct a full sewer study to address all sanitary issues relating to this project to the standards and specifications of the Sacramento Area Sewer District. Also determine sewer needs for build out;
- Consult with property owners on ultimate raised median locations;
- Prepare Streetscape Improvement Plans;
- Identify opportunity sites for Revitalization.

B. Funding Sources

Creation of a long-term mechanism for continued maintenance and security along the Boulevard is critical to the successful implementation of the Streetscape Master Plan. Without proper maintenance, the benefits provided by the streetscape improvements will be lost over time. Achieving an effective maintenance program can be accomplished either through the establishment of a Landscape and Lighting District and/or a Property-Based Business Improvement District.

Both funding sources are calculated on a linear front footage basis for each parcel. The linear front footage is the length of the property line adjacent to West Auburn Boulevard.

Landscape and Lighting District (LLD)

An LLD is an instrument that provides funds for the regular maintenance and upkeep of landscape areas, funded by an assessment fee assigned to all properties receiving the benefit of the improvements. Funds collected from an LLD are used solely for the maintenance and replacement of items within the district.

Implementation of an LLD depends on a majority of property owners voting to participate in the program. Existing owner-occupied single-family residential uses within the Plan area will be exempted from the Landscape and Lighting District (LLD).

Property-Based Business Improvement District (PBID)



Throughout the master planning process, a Property-Based Business Improvement District (PBID) was discussed with and encouraged by property owners. This Master Plan recommends the establishment of a PBID to provide consistent maintenance and security along the Boulevard in addition to supporting and identifying initial catalyst projects to further redevelopment along West Auburn Boulevard. Property owners must support and vote on acceptance of a PBID. This type of funding would provide property owners better control over the way their money is spent in that an elected Board of Directors oversees the PBID.

The completed Streetscape Improvement Plans will include details on the number and type of trees, plants, benches, streetlights, trash cans and other streetscape amenities so that an annual maintenance budget can be calculated. The annual maintenance budget will be divided by the total linear feet of West Auburn Boulevard within the Plan area. The resulting number will be the annual assessment per lineal front footage for each business.

C. Phasing Considerations

Phasing of large-scale projects is a requirement in today's economy. Phasing of construction projects provides property owners and the County the ability to hand select projects which meet their collective goals. Projects may be realized earlier and therefore be more consistent with the goals of this Master Plan.

Stakeholders were routinely asked what priorities were given to the recommended improvements. Stakeholder priorities identified visibility along West Auburn Boulevard and to the Boulevard from the Capital City Freeway as a component of Phase I streetscape and utility improvement construction. Every effort should be made to include the landmark blade signs in Phase I construction.

A very close second priority is the requirement to clean up the Boulevard. The construction of the "K-rail" separation between the freeway and the Boulevard, as well as the resulting curb, gutter and landscape improvements immediately adjacent to the "K-rail," would substantially realize this priority.

D. Projected Costs

In order to adequately address the phasing, funding and construction needs of a project a budget must be formulated for the proposed work. While this estimate has addressed all the elements outlined in the Streetscape Master Plan, it is impossible to identify all project requirements until engineered Streetscape Improvement Plans are produced.

The following is a budgeted estimate of construction costs based on the elements of the Streetscape Master Plan. This estimate is based on 2006 dollars and should be updated as Streetscape Improvement Plans are produced.

West Auburn Boulevard Streetscape Master Plan

Preliminary Budget Estimate

September 20,2006

Infrastructure

Right-of-Way Acquisition	300,000
Mobilization	850,000
Construction Staking	75,000
SWPPP/Pollution Control	80,000
Clearing and Grubbing	50,000
Traffic Control	100,000
Relocate BusStop	5,000
Undergrounding Utilities (Roadway)	2,022,500
Undergrounding Utilities (Site)	1,280,000
Roadway Excavation	200,000
Sidewalk/Driveway (Repairs)	182,500
ADA Ramp	60,000
Sidewalk/Driveway	73,500
Vertical Curb & Gutter - North Side of Street	437,500
Vertical Curb & Gutter - Median	513,500
Vertical Curb & Gutter - South Side	153,500
Vertical Curb & Gutter - Side Streets / Repairs	109,500
Storm Drain Additions	2,200,000
Signalization	250,000
Electrical Service	75,000
Water Service	150,000

Infrastructure Subtotal \$9,167,500

Surfacing & Finishes

Enhanced Median Turn Lane	1,059,500
Median Maintenance Border	23,000
Enhanced Paving at K-rail	995,500
Cold Plane AC Pavement	782,000
AC Overlay	1,173,000
Striping	122,000
Crosswalk	13,000

Surfacing & Finishes Subtotal \$4,168,000

Signage & Lighting

Miscellaneous Signage	50,000
Painted Street Lights/Traffic Signals	915,500
Landmark Blade Sign with Lighting	750,000
Corner Monument	59,500
Inventor Monument	256,000
Miscellaneous Painting	50,000

Signage & Lighting Subtotal \$2,081,000

Furnishings & Fencing



Decorative Fencing	1,039,500
K-Rail	2,188,000
Benches	31,000
Bike Racks	26,500
Bollards	35,000
Tree Grates	60,000
Trash Receptacles	15,000
Bus Shelter / Stop	30,000
Furnishings & Fencing Subtotal	\$3,425,000
Landscaping	
Shrubs & Groundcover	2,855,500
Trees	224,500
Pruning of Existing Trees	25,000
Topsoil Backfill	740,500
Finish Grading	71,500
Soil Amendments	28,500
Bark Mulch	198,500
48 Station Irrigation Controller	50,000
Water Meters and Backflow Devices	30,000
Irrigation	571,000
Landscape Maintenance Period (120 Days)	50,000
Landscaping Subtotal	\$4,845,000
Project Subtotal	\$23,686,500
Improvement Plans (10%)	2,368,500
Administration & Project Management (15%)	3,553,000
Inspection (3%)	710,500
Permits & Fees (2%)	473,500
Contingencies (20%)	4,737,500
Project Grand Total	\$35,529,500

SECTION SEVEN: Future Considerations



Section Seven identifies several planning issues that require further consideration by the jurisdictions and agencies that regulate West Auburn Boulevard. The ultimate success of the West Auburn Boulevard Streetscape Master Plan and Special Planning Area Ordinance depends on the resolution of these issues.

During the master planning process, several issues emerged which can only be addressed in conceptual terms at this time. However, these issues are critical to the ultimate success of the West Auburn Boulevard Streetscape Master Plan and Special Planning Area Ordinance and should be addressed by the County of Sacramento, the California Department of Transportation (Caltrans) and other involved agencies as quickly as possible.

A. Capital City Freeway Right-of-Way Landscaping

The California Department of Transportation has developed landscape plans for the Capital City Freeway which immediately abuts West Auburn Boulevard. Implementation of these plans was not funded and therefore will not proceed to construction at this time. The plans include removal of select trees within the Caltrans right-of-way, pruning of existing trees, and shoulder plantings within the right-of-way. As the plans currently stand, the plantings are not consistent with the goals of this Streetscape Master Plan. Any future project with Caltrans should be scrutinized closely to ensure that the plantings are consistent with the Master Plan, visibility to the Plan area and enhancement goals within the Plan area.

The improvements planned by Caltrans are crucial to the success of this Streetscape Master Plan, since the primary perception of West Auburn Boulevard occurs initially with the view from the freeway. Constituents who participated in the creation of this Master Plan remarked consistently that, without an active Caltrans project to improve its right-of-way, little work on the County's part would be worthwhile.

Caltrans, as part of the Technical Advisory Committee, is aware of these concerns and every effort should be made for the County and Caltrans to work together to seek funding for this project.

B. Howe Avenue/ Bell Street Ramp Closure or Reconstruction

Two "hook ramps" currently exist within the Plan area at Howe Avenue and Bell Street. These ramps place a significant impact on West Auburn Boulevard. Their alignment makes vehicular, pedestrian and bicycle traffic awkward and causes the western half of the Plan area to be under-used. In addition, knowledgeable freeway travelers often use these ramps as a bypass during peak freeway travel times. The County has researched the impacts of these ramps several times over the years and has approached Caltrans with their findings. However, Caltrans has no plans currently to close or improve either the Bell or Howe Avenue on/off ramps. This



Plan recommends that the ramps be realigned to improve the connection to West Auburn Boulevard and improve conditions for pedestrians and bicyclists. Should improvements proceed, the alignment of West Auburn Boulevard should be examined to encourage better traffic movement in the Plan area.

C. Fulton Avenue Overpass

The County, City of Sacramento and Caltrans recognize that the overpass at Fulton Avenue is presently substandard, although reconstruction of this overpass is not contemplated in the near term. This Master Plan considers the reconstruction of this overpass to current standards to be paramount to the success of the Plan area. The Fulton Avenue overpass is the only connection between the north side of the freeway and Haggin Oaks Golf Course and the Plan area. This link is vital to the economic future of the Boulevard and must be upgraded. Considerations in reconstruction should be addressed as follows:

- Ensure pedestrian safety and access on both sides of the overpass;
- Carefully examine the number of lanes and lane widths (particularly if ramp closures are eminent or have already occurred) with respect to traffic volumes;
- Respect existing landscape features (particularly the Fulton Avenue landmark element) in any redesign of on/off ramp reconstruction.

D. Consider Bike Lane Location Options

During the public workshops, stakeholders advocated against the placement of bike lanes along West Auburn Boulevard due to safety and maneuverability considerations. This requirement, however, is mandated by the County's Bikeway Master Plan and can be removed only by an act of the Board of Supervisors. This Master Plan recommends that flexibility along the Boulevard be fully recognized and the placement of bike lanes proceed. The bike lanes will allow for greater vehicular maneuverability within the Plan area.

E. Parcel Consolidation, Shared Driveways and Reciprocal Parking

The SPAO intends to encourage parcel consolidation, reciprocal parking and/or shared driveways. However, the present condition of the Boulevard and its adjacent parcels does not provide for this improvement, except in rare circumstances. As parcels are consolidated and driveway easements and reciprocal parking agreements are adopted, the County should research the ability to place additional raised and planted medians, consistent with this Master Plan, to further accentuate the value of the Boulevard.

Appendices

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Technical Memoranda

1. INTRODUCTION

This study evaluated the transportation conditions on Auburn Boulevard between Marconi Avenue and Watt Avenue in County of Sacramento.

Operation of the study roadway segments on Auburn Boulevard area were evaluated for the following scenarios:

1. Existing Conditions
2. Existing Plus Project Conditions
3. Future (year 2025) – No Project Condition
4. Future (year 2025) - Plus Project Condition

STUDY AREA

The following roadway segments were identified for inclusion in the analysis:

Roadway Segments:

1. Auburn Boulevard – Marconi Avenue to Howe Avenue
2. Auburn Boulevard – Howe Avenue to Bell Street
3. Auburn Boulevard – Bell Street to Fulton Avenue
4. Auburn Boulevard – Fulton Avenue to Business 80 eastbound off-ramp
5. Auburn Boulevard – Business 80 eastbound off-ramp to Watt Avenue

ANALYSIS METHODOLOGIES

Level of service (LOS) is a qualitative term that describes the operating performance of an intersection or roadway segment. LOS is measured quantitatively and reported on a scale from A to F, with “A” representing the best performance and “F” the worst. The County of Sacramento considers LOS F as unacceptable in urban locations. Table 1 describes level of service conditions.

TABLE 1	
LEVEL OF SERVICE DEFINITIONS	
Level of Service	Description
A	Represents free flow. Individual users are virtually unaffected by others in the traffic stream.
B	Stable flow, but the presence of other users in the traffic stream begins to be noticeable.
C	Stable flow, but the beginning of the range of flow in which the operation of individual users becomes significantly affected by interactions with others in the traffic stream.
D	Represents high-density, but stable flow.
E	Represents operating conditions at or near capacity level.
F	Represents forced or breakdown flow.

Roadway Segments

Roadway segment LOS is based on the roadway type, number of lanes, and daily volume of the segment. The LOS thresholds for arterial roadways are presented in Table 2.

TABLE 2					
ROADWAY SEGMENT LEVEL OF SERVICE/VOLUME THRESHOLDS – SACRAMENTO COUNTY					
Facility Type	Maximum Volume for Given Service Level				
	A	B	C	D	E
2-Lane, Residential Collector with Frontage (2R)	1,600	3,200	4,800	6,400	8,000
2-Lane, Low Access Control Arterial ¹ (2L)	9,000	10,500	12,000	13,500	15,000
2-Lane, Moderate Access Control Arterial ² (2M)	10,800	12,600	14,400	16,200	18,000
2-Lane, High Access Control Arterial ³ (2H)	12,000	14,000	16,000	18,000	20,000
4-Lane, Low Access Control Arterial (4L)	18,000	21,000	24,000	27,000	30,000
4-Lane, Moderate Access Control Arterial (4M)	21,600	25,200	28,800	32,400	36,000
4-Lane, High Access Control Arterial (4H)	24,000	28,000	32,000	36,000	40,000
6-Lane, Low Access Control Arterial (6L)	27,000	31,500	36,000	40,500	45,000
6-Lane, Moderate Access Control Arterial (6M)	32,400	37,800	43,200	48,600	54,000
6-Lane, High Access Control Arterial (6H)	36,000	42,000	48,000	54,000	60,000

Note: Facility Type Arterials:
¹Low Access Control is defined as having more than 4 stops/mile, frequent driveways, and 25-35 mph speeds.
²Moderate Access Control is defined as having 2-4 stops/mile, limited driveways, and 35-45 mph speeds.
³High Access Control is defined as having 1-2 stops/mile, no driveways, and 45-55 mph speeds.
 Source: *Traffic Impact Analysis Guidelines*, County of Sacramento, July, 2004

2. EXISTING CONDITIONS

This chapter provides a description of Existing Conditions on Auburn Boulevard in the study area.

ROADWAY SYSTEM

Regional access to Auburn Boulevard is provided by Business 80 (State Route 51). Study area roadways include Auburn Boulevard, Fulton Avenue, Watt Avenue, and Howe Avenue.

Business 80 is a north-south freeway that extends from US 50 in Sacramento to I-80. In the vicinity of the project it is a four-lane limited-access urban freeway. Full or partial interchanges at Marconi Avenue (partial), Howe Avenue (partial), Bell Street (partial), Fulton Avenue (full), and Watt Avenue (full) provide access to the Auburn Boulevard in the study area.

Auburn Boulevard is a two- to four-lane arterial that runs east-west from Sacramento to Citrus Heights. In the project area Auburn Boulevard parallels Business 80 and is used to avoid congestion on Business 80 during the AM and Pm peak hours.

Fulton Avenue is a four-lane road that runs north-south from Fair Oaks Boulevard to Business 80. It has a full interchange with Business 80.

Watt Avenue is a four- to six-lane thoroughfare that runs north-south from Placer County to Elk Grove (as Elk Grove-Florin Road). In the study area Watt Avenue has six lanes and has a full interchange with both Business 80 and I-80.

Howe Avenue is a two- to six-lane north-south roadway beginning at Sheldon Road (as Power Inn Road) and extending north to Business 80. In the study area it is a two-lane facility and has a partial (eastbound on and off-ramps only) interchange with Business 80.

TRAFFIC VOLUMES AND LEVELS OF SERVICE

Daily traffic counts were collected by Sacramento County for the Howe Avenue to Bell Street and Fulton Avenue to Business 80 eastbound off-ramp segments. The traffic volumes on the Bell Street to Fulton Avenue and Business 80 eastbound off-ramp to Watt Avenue segments were estimated using existing traffic volume data for adjacent segments and 2002 Base Year SACMET traffic model forecasts.

The average daily traffic volumes and level of service estimates for the study roadway segments are summarized in Table 3. All of the roadway segments are operating at LOS A.

TABLE 3			
AUBURN BOULEVARD ROADWAY SEGMENT LEVELS OF SERVICE - EXISTING CONDITIONS			
Road Segment	Facility Type¹	ADT²	LOS³
Howe Ave. to Bell St.	2M	2,600	A
Bell St. to Fulton Ave.	3M	5,100 ⁴	A
Fulton Ave. to Business 80 eastbound off-ramp	3M	12,500	A
Business 80 eastbound off-ramp to Watt Ave.	4M	20,300 ⁴	A

Notes: 1. Facility Types are described in Table 2
 2. ADT = Average Daily Traffic (24 hour weekday)
 3. LOS = Level of Service
 4. Estimated daily traffic volume
 The LOS analysis was based on capacities presented in the County of Sacramento's *Traffic Impact Analysis Guidelines, 2004*
 LOS is based on roadway type, number of lanes and average daily traffic volume.

3. EXISTING PLUS PROJECT CONDITIONS

PROJECT DESCRIPTION

The Auburn Boulevard Streetscape Master Plan is a comprehensive plan to guide streetscape development along the boulevard. The goals of the Master Plan are to create a strong, recognizable community identity; create a safer environment for pedestrians, vehicles, bicycles, and business owners; increase visibility; slow vehicle speed; encourage a variety of business ventures; and strengthen urban form. To that end portions of the roadway that have multiple lanes (between Bell Street and the Business 80 eastbound off-ramp) would be converted to a two-lane arterial with a painted or raised median and bicycle lanes. The pedestrian environment would be improved by way of upgraded sidewalks and street lighting. A new traffic signal would be installed at the Auburn Boulevard/Morse Avenue intersection.

In the section of Auburn Boulevard between Marconi Avenue and Bell Street the existing two-lane section would be retained, but bicycle lanes would be added. The pedestrian environment would be improved by way of upgraded sidewalks and street lighting.

TRAFFIC VOLUMES AND LEVELS OF SERVICE

The average daily traffic volumes and level of service estimates for the study roadway segments with the completion of the project are summarized in Table 4. With implementation of the proposed project all of the roadway segments are projected to be operating at LOS B or better.

Road Segment	No Project			Plus Project		
	Type ¹	ADT ²	LOS ³	Type	ADT	LOS
Howe Ave. to Bell St.	2M	2,600	A	2M	2,600	A
Bell St. to Fulton Ave.	3M	5,100 ⁴	A	2M	5,100 ⁴	A
Fulton Ave. to Business 80 eastbound off-ramp	3M	12,500	A	2M	12,500	B
Business 80 eastbound off-ramp to Watt Ave.	4M	20,300 ⁴	A	4M	20,300 ⁴	A

Notes: 1. Facility Types are described in Table 2
 2. ADT = Average Daily Traffic (24 hour weekday)
 3. LOS = Level of Service
 4. Estimated daily traffic volume
 The LOS analysis was based on capacities presented in the County of Sacramento's *Traffic Impact Analysis Guidelines, 2004*
 LOS is based on roadway type, number of lanes and average daily traffic volume.

4. CUMULATIVE (2025) CONDITIONS

This chapter analyzes the transportation conditions for the Cumulative (2025) Conditions. Cumulative (2025) Conditions are identified for with and without project scenarios.

CUMULATIVE (2025) TRAFFIC VOLUMES

The SACMET TDF model was updated to estimate future traffic in the vicinity of the project. The TDF model incorporates land use developed from area general plans and Tier I 2025 Metropolitan Transportation Plan roadway improvements.

CUMULATIVE IMPACT ASSESSMENT

Roadway Segment Impacts

The Cumulative (2025) traffic volumes and roadway segment level of service analysis are summarized in Table 5. The conversion of Auburn Boulevard, in the study area does not result in any roadway segment operating at unacceptable levels of service (worse than LOS E). Implementation of the proposed project would result in the roadway segment between Fulton Avenue and Business 80 eastbound off-ramp degrading from LOS B to LOS D. All of the other segments are projected to operate at LOS A with or without the project.

Road Segment	Existing						2025 Conditions					
	No Project			Plus Project			No Project			Plus Project		
	ADT ¹	LOS ²	Type ³	ADT	LOS	Type	ADT	LOS	Type	ADT	LOS	Type
Howe Ave. to Bell St.	2,600	A	2M	2,600	A	2M	3,000	A	2M	3,000	A	2M
Bell St. to Fulton Ave.	5,100 ⁴	A	3M	5,100 ⁴	A	2M	5,600	A	3M	5,600	A	2M
Fulton Ave. to Bus 80 EB off-ramp	12,500	A	3L	12,500	B	2M	14,700	B	3L	14,700	D	2M
Bus 80 EB off-ramp to Watt Ave.	20,300 ⁴	A	4M	20,300 ⁴	A	4M	21,900	A	4M	21,900	A	4M

Notes: : 1. ADT = Average Daily Traffic Volumes (24 hour weekday)
 2. LOS = Level of Service
 3. Type = Facility Type (See Table 2)
 4. Estimated daily traffic volume
 The LOS analysis was based on the County of Sacramento's *Traffic Impact Analysis Guidelines*, 2004
 LOS is based on roadway type, number of lanes and average daily traffic volume.



MARK THOMAS & COMPANY, INC.
Providing Engineering, Surveying and Planning Services

Memorandum

To: Ms. Christine Anderson

File: 55-0306B

C:

From: Adrian Engel

Date: 7-13-2006

Re: Auburn Blvd Streetscape Master Plan

Technical Issues with Auburn Blvd Master Plan

Mark Thomas & Company, Inc. has been contracted to provide input on the civil engineering issues pertaining to the Auburn Blvd Master Plan. The project limits of the Study are along Auburn Blvd from west of Howe Ave to Watt Ave. The Master Plan is being produced in conjunction with the Auburn Blvd Special Plan Area Study. With the improvements one existing lane will be removed resulting in one traveled lane in each direction, a dual left turn lane, bike lanes and sidewalk on the south side of the street. The proposed streetscape improvements include landscaping, street printing, improved ADA facilities, and concrete barriers. This memo will address the technical challenges with implementing the streetscape improvements.

Caltrans Facilities

The project is adjacent to Route 51 Capitol City Freeway including four interchanges at Bell Ave, Howe Ave, Fulton Blvd and Watt Ave. The northern boundary between County and State right of way along of Auburn Blvd is currently defined by an earthen swale and chain link fence. Caltrans standards such as the Highway Design Manual and the AASHTO Roadside Design Guide will control the ultimate configuration of the ramps and the concrete barrier in the Master Plan.

Hook Ramps

The interchanges at the Bell Ave and Howe Ave consist of eastbound on and off hook ramps. Currently Caltrans does not support the use of hook ramps on new

projects due to the increased accident potential. Hook ramps historically can have short stopping sight distance, potential for wrong way movements, short acceleration/deceleration distance and/or inadequate superelevation. All of these factors can lead to increased accident rates. Taking this into consideration, any modification to the existing hook ramps may trigger considerable improvements which could escalate the cost of the Streetscape Master Plan significantly. The improvements could range from ramp lengthening and reconstruction to building completely new interchanges with massive right of way impacts. There are ongoing studies with Caltrans looking at interchange options in the area.

Right of Way Fence / Barrier

As part of the Streetscape Master Plan, the existing chain link fence along the Caltrans right of way is being replaced with a concrete barrier (type 60) and decorative fencing. The barrier and fence combination will only be 6 feet tall so it will not obstruct the view of the businesses along the south side of Auburn Blvd but will be more attractive and easier to maintain than the existing chain link fence, see the attached sketch. The grade difference is as much as five feet between the edge of pavement of the freeway and the pavement of Auburn Blvd. The grade difference between the roadways will need to be addressed with the final design of the concrete barrier with input from Caltrans review. Also the drainage swale will need to be reformed within the state right of way or replaced with a storm drain system.

Utilities (Placing under ground)

One component of the Streetscape Master Plan is to underground overhead utilities. There is a high voltage power line on the north side of Auburn Blvd that cannot be placed underground due to its high voltage. There are however both lower voltage electrical and communication lines on the south side of Auburn Blvd that have the potential to be placed underground. Due to the high cost of undergrounding utilities the County should look at funding strategies to distribute the cost. A potential strategy is to underground the utilities adjacent to the south side Auburn Blvd with the Master plan improvements and encourage the developers to underground the onsite utilities with redevelopment.



August 9, 2006



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Christine A. Anderson, ASLA
Senior Associate Landscape Architect
The HLA Group, Landscape Architects & Planners Inc.
1990 Third Street, Suite 500
Sacramento, CA 95814

SUBJECT: Draft Auburn Blvd. SPAO and SMP Document Comments

Dear Ms. Anderson:

Board of Directors
Representing:

County of Sacramento

City of Citrus Heights

City of Elk Grove

City of Folsom

City of Rancho Cordova

City of Sacramento

The West Auburn Boulevard Special Planning Area Ordinance (SPAO) and Streetscape Master Plan (SMP) corridor is located within the County Sanitation District 1 (CSD-1) service area boundaries. CSD-1 has reviewed the Draft SPAO and SMP document and has the following comments:

The addition of multi-use and multi-story facilities may increase the demand on the current sanitary sewer infrastructure in the subject corridor. Preliminary analysis shows that the sanitary collector and trunk sewer facilities currently serving this corridor may be capacity constrained. Therefore, as part of your preliminary analysis to redevelop this area, CSD-1 recommends conducting a sewer study to address all sanitary sewer issues relating to the subject project. The study should either demonstrate capacity in the existing system, or propose construction of new facilities to mitigate the project's impact on the existing system.

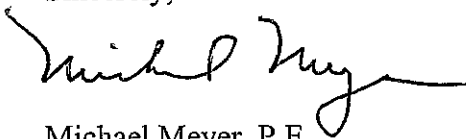
If you have any further questions or require additional information please feel free to call me at 875-7123.

Mary K. Snyder
District Engineer

Christoph Dobson
Acting Collection Systems Manager

Wendell H. Kido
District Manager

Marcia Maurer
Chief Financial Officer

Sincerely,

Michael Meyer, P.E.
Policy and Planning

cc: Roy Carlson, CSD-1 Capacity Management



Zoning Code of Sacramento County
DEPARTMENT OF TRANSPORTATION
DISTRICT 3 – SACRAMENTO AREA OFFICE
VENTURE OAKS – MS 15
P.O. BOX 942874
SACRAMENTO, CA 94274-0001
PHONE (916) 274-0635
FAX (916) 274-0648
TTY (530) 741-4509

*Flex your power!
Be energy efficient!*

September 8, 2006

Ms. Christine Anderson
The HLA Group
1050 20th Street, Suite 200
Sacramento, CA 95814

Dear Ms. Anderson:

Thank you for the opportunity to review the draft West Auburn Boulevard Streetscape Master Plan & Special Planning Area Ordinance written for Sacramento County and Sacramento Housing and Redevelopment Agency. The document does a nice job of reflecting the recommendations and strategies that were discussed at the public workshops and the Technical Advisory Committee meetings.

The recommendations for improvements that will require Caltrans approval are all conceptually feasible, though each will take time and non-Caltrans funding, which is recognized. The recommendations include improved maintenance (page 4.4), fence replacement (pages 4.3, 4.8, 4.12, and 5.2), crosswalks @ Fulton Ave. (page 4.13), landscaping coordination/improvements (page 6.1) and freeway ramp improvements (pages 6.1 and 6.2). It is appreciated that the text notes that Caltrans currently has no plans to alter or close any of the freeway hook ramps within the project area.

As the improvement of West Auburn Boulevard moves forward, Bruce de Terra (916) 274-0614 is the appropriate Caltrans Transportation Planning contact to coordinate any potential maintenance agreement, k-rail fence design approval, crosswalk installation by freeway ramps, landscaping, or ramp operations study.

Caltrans looks forward to continuing as a member of this project's Technical Advisory Committee and to working with the community to improve this stretch of Auburn Boulevard.

Sincerely,

A handwritten signature in cursive script that reads "Alyssa Begley".

ALYSSA BEGLEY
Associate Transportation Planner

c: Bruce de Terra

**Meeting Agendas and
Minutes; Presentation
Graphics**

Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Auburn Boulevard Stakeholders Meeting
October 27, 2005

Agenda

Introductions

Welcome

Project Background

Project Vision and Goals

Project Process

- Project Participants
- Roles
- Milestones

Discussion

- Lessons learned from Fulton Avenue
- Issues for Auburn Boulevard

Adjourn

Our next meeting is tentatively scheduled for January 18th at 1pm at the Clarion Hotel. Please make a note of it.



Troy Givans
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 givanst@saccounty.net



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) & Streetscape Master Plan
Auburn Boulevard Stakeholders (ABS) 1st Meeting

Project Number: 53505.01

Meeting Date: October 27, 2005

Meeting Time: 1:00 P.M.

Attending: See sign in sheet

Recorded By: Susan Collopy

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Donna Melendez with Sacramento Housing and Redevelopment Agency (SHRA), Troy Givans with the County of Sacramento and Christine Anderson with The HLA Group are co-project managers on this project.

Goals

- To create a plan and a blueprint for the future.
- To capitalize on the successes of Auburn Blvd.
- The process used for this project is a test case with the goal of changing the way the county does business.
- Streamline the process – encourage quality development
- The Special Planning Area Ordinance & the Streetscape Master Plan will be policy documents that will promote these goals.

Stakeholder Role in the process

- Provide information to the project team on the needs of the area.
- Identify what needs to be done to be successful.
- Provide ideas and questions to the project team.
- Form consensus and generate support in the community.
- Attend public workshops.

Technical Advisory Committee: Consists of staff members from the County, SHRA and Department of Environmental Review and Assessment (DERA) and others.

Project Timeline

- Two community workshops (design charettes) scheduled for February and March.
- Three ABS stakeholder meetings (this is the first meeting of three).
- Expedite the SPA document in front of the Streetscape Master Plan. Expect to submit the SPA document to the County Board of Supervisors in early fall of 2006.
- The Streetscape Master Plan will be completed shortly after the SPA document.
- Supervisor Peters commented that she would like to request a more aggressive schedule, with less time between the workshops and document submittal.

Products

- Special Planning Area Ordinance – This is a policy document that will provide specific planning direction for this area. The document needs to be completed in order to move forward with projects that will move through the Planning Department.



- Streetscape Master Plan – This will be a document which will provide design guidelines and specific design recommendations for the streetscape along Auburn Blvd.
- Newsletters about this project will be going out to all owners/residents located within the 500' project limit line.

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Feedback on Fulton Avenue Streetscape Renovation

- Cooperation with the County was excellent.
- County follow through was very slow – took 6 years to complete landscaping.
- The scope of work for Fulton Boulevard was different than the scope of work for Auburn Boulevard.

Comments from the Stakeholders

- Suggested inviting more people to the Design Workshops (beyond the 500' project limit line) and do more outreach in the community so more people know about the project early on. Goal is to prevent people from joining in at the end of the process with complaints.
- Invite specific Homeowners Groups and Local organizations (check with Howard Schmidt at Supervisor Peters' office for information on local organizations.)
- Commented on the need to provide continuous education of the SPA ordinance and the Streetscape Master Plan to the public.
- Supervisor Peters to mention the project and Design Workshops in her newsletter to her constituents.

Key Issues from Stakeholders

- Currently there are jurisdictional problems due to different agencies that have jurisdiction in the area (County of Sacramento, City of Sacramento and Caltrans). The south side of the street is in the County, at the centerline it is within the City of Sacramento limits, and at the existing chain link fence it becomes Caltrans right of way. It is unclear who has responsibility over certain tasks, people call with problems and are told to call someone else. Problems with abandoned cars. Need to know who to call with problems.
- The chain link fence is poorly maintained. Stakeholders want visibility from the freeway.
- Consider a long term lease from Caltrans, then take over landscaping and maintenance. Project team to follow up on this for the next ABS meeting
- Consider a maintenance agreement with the City of Sacramento.
- Caltrans should be involved in this process.
- Caltrans background: Some existing on and off ramps are problematic, there is no current plan to revise them, revising them could require an EIR.
- Issues to discuss with Caltrans: existing chain link fence and maintenance.
- Consider upgrading the Caltrans fence and look for opportunities to add artwork or sculpture to the fence. Caltrans has done this in other places. Project team to follow up.
- Crime had improved and is now backsliding some (it is better than it was). Problems with vandalism, panhandling, and prostitution.
- Suggested to follow up with the POP officer Paul Pace (876-5744) with crime concerns.
- Bobbe Dworkis (876-5241) is the Service Area Manager with Sacramento County Neighborhood Services at Marconi station. She can help coordinate intergovernmental issues.
- Consider coordinating private security with public security with the creation of a Property-Based Business Improvement District (PBID).
- Currently Fulton Avenue has private security that is working well, Auburn Blvd. doesn't have this in place.
- Costs for security on Fulton about \$100,000 per year.
- Look for ways to improve security on Auburn Blvd.



- Property owners have had problems getting business group started.
- Bike Trail- there is no current connection to the existing bike trail located north of I-80. There has been no discussion regarding bike paths on Auburn Blvd., not an ideal location for bike travel. Project team will check with transportation agencies regarding any need for bike lanes.
- Airport Noise from McClellan – shouldn't be an issue, there are some complaints about helicopters. Project team will check up on and give update.
- ABS member request for a copy of the large aerial photo of Auburn Blvd. with property info. on it.
- Streetscape issues- Medians are a concern because they limit access into businesses
- Consensus is that people prefer separated sidewalks (there is an existing strip of landscaping between the sidewalk and the curb). Currently some sections of Auburn Blvd. have separated sidewalks.
- Make sure business owners are informed of any plans to install landscape medians because these have a direct impact on businesses. Business owners would prefer no medians.
- If the area adjacent to the freeway can be landscaped, may not need landscape medians.
- What is the potential for undergrounding overhead utility lines? Large power lines on the North side of the street cannot be undergrounded, undergrounding may be possible for the other smaller lines on the South side of the street.
- Thoughts regarding on-street parking – useful for providing employee parking. Should be daytime only. No parking at night to discourage truck parking. Also consider land purchased to create public parking lots.
- Consider if on-street parking is at odds with beautification goals.
- Speed is an issue – people drive too fast, employees have had car accidents.
- Currently there is nothing to slow drivers down.
- Separate Auburn Blvd. from the freeway aesthetically.
- Traffic – currently traffic is bad, backs up in some areas at rush hour, a lot of people using Auburn as a way to avoid the freeway.
- Lane Configuration: when Auburn Blvd. was two lanes each way with no center lane there were a lot of accidents. Project team to provide examples of alternative lane configurations.
- Will the SPA address on site landscape requirements? Yes, staff needs input on this from the ABS members and this will also be a topic at the Design Workshops.
- The SPA will address landscape coverage, building coverage, and signage.
- What about projects in progress when the SPA is approved? Will they be required to comply with new guidelines? Projects in progress will be reviewed on a case by case basis.
- Zoning Comment: Is “Form Based Zoning” possible to be used for this project? (Form based Zoning places emphasis on the physical forms allowed for an area, and moves away from “use based zoning”.) Project team will look into this.
- Streetlighting: not enough, doesn't always work, turning on and off at the wrong times
- Design Comment: Consensus is that the design and image of Auburn Blvd. should be different from Fulton Blvd.
- Auburn Blvd. is practical, convenient, and accessible. People come to Auburn Blvd. for a reason. Keep this in mind while developing design theme.
- Consider an industrial theme, use of sculptural elements, or landmarks to identify location.
- Palm Trees – good because they don't block views, no strong opinions yes or no.
- Oak Trees – grow very slowly, block views.

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Meeting adjourned at 2:30 pm.

Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Technical Advisory Committee Meeting
November 17, 2005

Agenda

Introductions

Welcome

Project Background

Project Vision and Goals

Project Process

- Project Participants
- Roles
- Milestones

Discussion

- Issues for Auburn Boulevard
- Q & A

Adjourn

Our next meeting is tentatively scheduled for December 8th at 9:30am at the County Administration Office. Please make a note of it.



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) & Streetscape Master Plan
Technical Advisory Committee (TAC) 1st Meeting

Project Number: 53505.04

Meeting Date: November 17, 2005

Meeting Time: 9:30 A.M.

Attending: See sign in sheet

Recorded By: Susan Collopy

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General Comments

- A contact list will be provided to TAC members so that TAC members can contact each other directly.
- County Supervisor Susan Peters is very supportive of this project. She would like to see it completed as soon as possible.
- Binders have been provided to all TAC members. They were designed to hold additional information you will receive as we move through the process.
- Standard meeting format will be to distribute questions to TAC members prior to each meeting, please review questions prior to the meeting and be prepared to discuss questions and provide answers at the meeting.
- Requesting TAC members to bring up any issues you have regarding this project either to the project team members or at the TAC meetings.

Background

- Most of the area is in the Auburn Redevelopment area. Auburn Boulevard was a thriving commercial area, then it declined, it has improved significantly in the past few years. Currently there is development interest from businesses who are attracted to the area for its visibility and large parcel size.
- Auburn Blvd. Stakeholders (ABS) This committee includes property owners and other interested parties. The role of this committee is advisory, to provide information and feedback, and to garner public support for the project.

Project Goals

- Use this project as an opportunity to raise the bar, encourage and facilitate quality projects.
- Allow for more flexible zoning to eliminate planning hurdles.
- Provide specific design criteria to improve the quality of future projects.
- Compete for future public funding for additional projects on Auburn Blvd.
- Goal is to have final SPA document in place and approved by fall 2006.
- Role of the TAC is to provide information and discuss and resolve issues relevant to this project in a quick and timely manner.



Response to Questions sent out prior to meeting:

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Cal Trans

1. What options do we have for a new fence configuration to separate the Highway from Auburn Boulevard? Particularly with a 2-3' grade separation.

Answer:

- Yes, a new fence is a possibility. The fence would need to go through the Caltrans design review process, Cal Trans TAC members could coordinate that.
- Any new fencing would not be funded by CalTrans.
- Current Caltrans project to relandscape and irrigate from Howe Avenue to Watt Avenue including Watt Interchange. No new fence is planned with this project. There is no money available in the funding of this project for new fencing.
- Larger scale shrubs will be planted on freeway frontage in front of residential areas.
- Shrub species include oleanders, redbuds, arctostaphylos densiflora.
- Existing Modesto Ash to be removed as part of project (Howe to Bell)
- Clear Recovery Zone adjacent to the freeway – no trees are allowed in this area.
- Project Timing – If it is approved next month, construction will be in 2006. Project could be pulled from the schedule, don't know yet what will happen. Project is on the California Transportation Commission agenda – the Hearing Date is December 17th.
- Requested a copy of the project report and Plans, Specifications and Estimates be given to Christine Anderson (on the project team).
 - Need to review Visibility Goals for this project.
 - Would need a change order once it is approved to alter project design.
 - Project is considered a rehabilitation project (no EIR needed).
 - Revising plant species won't be too expensive.
 - Altering location of plants would be much more expensive.
- Request for outreach from Cal Trans staff to ABS. Can Cal Trans TAC members attend the next ABS meeting?

2. Can existing trees within the Cal Trans right-of-way be pruned for visibility?

Answer:

- Some of the trees are coming out. Yes, trees can be pruned.
- If the project is approved in December, the TAC could review the plans, request revisions, and review proposed revisions with the ABS committee and CalTrans.

3. Can a maintenance agreement (Memorandum of understanding - MOU) be worked out between Cal Trans and the County to maintain landscape easements? If so, what is the process for this?

Answer:

- Yes, the County would maintain, usually a 20 year agreement.
- Alyssa Begley will provide Troy Givans information and contacts for researching a MOU, including timeline for MOU completion.
- CalTrans will typically will not allow work in their right-of-way (ROW) even if it is on the other side of the fence from the freeway. An



encroachment permit is needed to do work in the CalTrans right of way.

- Property line is unclear – Cal Trans starts at back of curb.
 - Clarify ROW, fence location
 - Suggestion by Caltrans to submit an encroachment permit request, get process started (ROW department takes a long time) then right of way issues would be researched.
- Rob Himes requested ROW maps.
- Sidewalk on the north side of Auburn Blvd. is being considered to provide access from the existing bus stop on the north side to the south side of Auburn Blvd. and also for aesthetics. Currently there is no sidewalk at the bus stop and it is a safety hazard.

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4. Will Cal Trans restrict signage placed on County or private property, but within view of the highway? If so, how?

Answer:

- Signage Restrictions are based on CalTrans document– Outdoor Advertising Act and Regulations. Given to David Wade.
- Alyssa Begley will forward information on this document and CalTrans contact information regarding these regulations. Copy given to David Wade.
- Gateway policy can go forward, maintenance is an issue, need an MOU
- Fence can not be wrought iron or any custom fence
- Chainlink fence (type 6 or 7) preferred, fence can have a color
- Could put whatever you wanted on the other side of fence.
- Block wall will also be allowed per CalTrans
- ABS stakeholders not fond of Block Wall – will block views.
- Block wall costs – approx. \$300.00 linear foot
- Could have artwork on fencing, but it would require a maintenance agreement (Caltrans will not maintain custom fencing).

DERA

5. What length of time can we expect for review from the Department?

Answer:

Until project is more well-defined, can't estimate timeline.

6. What kinds of items might slow down the process?

Answer:

- Federal funding – CEQA/NEPA would slow process
- A Negative Declaration will take 3 to 4 months
- Submitting one document instead of two (SPA and SMP) might streamline the review process
- It would be helpful to provide as much background info up front as possible
 - Arborist Report
 - Noise Analysis
 - Other studies
- Jim Schubert noted that the county could help out with preparation of the arborist report.



7. We are currently providing a 500' mailing radius with the addition of neighborhood associations and other interested parties; will this be acceptable?

Answer:

- Project team to coordinate with Bobbe Dworkis and Howard Schmidt for information on neighborhood associations and other interested groups.
- Supervisor Peters will prepare an article for the Newsletter
- Newsletter will also be posted on the Sacramento Housing and Redevelopment web site .

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Planning

8. We would like a copy of the County's Draft Design Guidelines.

Answer:

- No County planning staff present at the meeting.

Transportation

9. Can we get a sense of the history of lane reconfiguration and on-street parking for the Boulevard?

Answer:

- The lane configuration has not changed for a while on Auburn Blvd.

10. Has the County given any consideration to reconfiguration of the on/off ramps at Howe and Bell? If so, what configurations have been discussed?

Answer:

- Some studies have been done, but there are no current plans to alter off-ramps.

11. We noticed during a field walk that there are several occurrences where drain inlets have been placed on private property and tied to the County's street storm drain. Theoretically this is due to the fact that several properties are actually lower than the roadway. Is this an acceptable practice? Could this be an option for future property development if their sites are low?

Answer:

- Direct this question to Water Resources, in general this is not a preferred practice but they have allowed it in the past.

12. Bus stops on the north side of Auburn currently have no feasible means of pedestrian access. What does the County envision as being an appropriate response to this item?

Answer:

- Sidewalks are feasible in this area, mid-block pedestrian signals are not in favor. A mid-block crosswalk will not be allowed without a signal. Transportation can look into this request.
- 2-lane road – can have crosswalk
- 4-lane road – no crosswalk without signal
- Review situation with RT

13. Are there bike routes planned on or through the Auburn Blvd. corridor? Does the County have any plans to link up to the existing City bike trail north of the freeway at Haggin Oaks?

Answer:

- Bike routes – Yes. See County's 2010 Bikeway Master Plan – Requested a copy, also available on the County's website.



- The bikeway master plan doesn't address connecting to the bike path at Haggin Oaks.
- Could provide a connection to Fulton Avenue.

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14. We have heard that there are many reports of speeding along the Boulevard; can the County confirm this? Can we get accident reporting? When was the last traffic count?

Answer:

- County can provide data on speeds. Troy Givens to get back to Stephen White regarding conducting a speed study.
- County will provide accident data.
- Traffic counts traveling West to East on Auburn Blvd.
 - 2635 at Howe
 - 12,473 at Fulton
 - 25,581 at Watt

15. What is the County's understanding of street lighting along the corridor? Many property owners are complaining that it is inadequate in many locations.

Answer:

- County will review current lighting and get back to the TAC with information.

16. Does the County have any feelings regarding the placement of medians along the Boulevard?

Answer:

- In general medians provide an aesthetic and safety benefit
- Issues
 - Left turn access into businesses will be lost
 - Maybe additional ROW requirements at intersections if medians are added
 - Issues with sewer pipes, don't like sewer pipes under medians. As built are available showing the location of existing sewer pipes.
- Have County information, Cal Trans project report has some utilities info.

17. Would the County be amenable to providing on-street parking in particular areas?

Answer:

Need to have more specific information before they can make a determination. There will be bike lanes on Auburn Blvd which may pose a conflict with on street parking.

18. How can we get Auburn Blvd. on the SMUD's list of corridors for funding to underground powerlines.

Answer:

- Bill Slatton, SMUD Board of Directors is the person to contact regarding the undergrounding list.
- 2 levels of power lines on Auburn Blvd.
- High power lines – too expensive to underground
- Would like to look into undergrounding the smaller power lines
- Can we relocate power to the back of the property?
 - Look into, check with Bill S.



19. What types of special paving will the department allow within the right-of-way?

Answer:

- At crosswalks, pavers, or stamped asphalt is allowed, open to different types of paving.
- Consider using a different color asphalt for bikes lanes (using it in Citrus Heights).
- See the county Pedestrian/ADA Master Plan Ped/ADA Master (see bikeway color in this document).
- Note that Utilities won't repair special paving.

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20. How much ADA compliance will we need to accommodate with this project?

Answer:

- Need to comply wherever you are working .
- If there is work in part of an intersection, entire intersection must be ADA compliant.

21. Is there a need or are there plans to make any traffic signal upgrades?

Answer:

- No plans right now.
- Signal light at Auburn and Morris won't happen anytime soon, it is #38 in priority on the Sacramento Transportation traffic priority plan. Contact person regarding this is Lupe Rodriguez.

Neighborhood Services

22. We have already heard several complaints about trash collection along the fence line, non-conforming signs, illegal parking and dumping (including abandoned cars), and crime in the area. We are also aware that the north side of Auburn Boulevard falls in the City's jurisdiction. Would the County be amenable to a long-term maintenance agreement with the City to provide services for this area? And if so, what would be the process for this?

Answer:

- Need to identify services the services in question, contact the departments responsible and develop a MOU to be approved by the City of Sacramento and the Board of Supervisors.
- Requested Bobbe Dworkis (Sacramento Area Manager, Sacramento County Neighborhood Services) attend the next few ABS stakeholder meetings.
- Will send out tentative ABS meeting schedule to Bobbe to review for any scheduling conflicts.

General Comments

- A landscape and lighting district (LLD) was formed to maintain the landscaping on Howe Avenue. Must have a vote greater than 50% to adopt LLD.
- If a LLD is formed for Auburn Blvd., mobile home parks could be exempt so residents would not be charged.
- Creation of a PBID was discussed at the ABS meeting – mostly for security patrols.
- There is a sewer interceptor that crosses the highway and runs down Bell, there will be restrictions on surface improvements adjacent to the interceptor.



Items to Follow up on

- Existing housing on Auburn will not be removed, no additional housing will be proposed with this project.

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- Information requested from Cal Trans and Transportation.
- Minutes will be provided for this meeting
- Please review the tentative meeting schedule and review for any conflicts. There are 6 TAC meetings total.
- Christine Anderson to work with Bobbe Dworkis on scheduling the public meetings.

Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Auburn Boulevard Stakeholders Meeting
December 15, 2005
1:00pm

Agenda

Welcome / Introductions

Review of Minutes from October 27th meeting

Round Table Discussion

Planning Options

Streetscape Options

Charette Venue

Adjourn



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) 7 Streetscape Master Plan
Auburn Boulevard Stakeholders (ABS) 2nd Meeting

Project Number: 53505.01

Meeting Date: December 15, 2005

Meeting Time: 1:00 PM

Attending: See sign in sheet

Recorded By: Susan Collopy

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Summary of Meeting:

Introduction

Review of minutes from Meeting #1

Items on the minutes are being followed up.

Caltrans issues:

Caltrans has no set maintenance schedule; they maintain the landscaping along I-80 at Auburn Boulevard on an “as needed” basis.

- A proposed project to renovate the Caltrans landscape along Auburn Blvd. has been designed, but is now on hold due to lack of funding.
- Looking into trying to get this Caltrans project started up again.

Roundtable Discussion, led by David Wade:

- His comments are focusing on the issues related to the SPA (Special Planning Area) document which affects the area from the back of curb to the rear property line of properties located on Auburn Blvd.
- The goal of the SPA document is to alter the existing zoning to make it easier to get things done.
- Current zoning regulations for this area are being reviewed, allowed use and requirements will be changing to encourage economic development along Auburn Blvd.
- Form based zoning is being considered for this area. Form Based Zoning places requirements on the physical forms (buildings, landscaping, etc.) rather than focusing on uses. It is a more visual approach to planning that encourages uses rather than limiting them.

Questions Posed to ABS Members by David Wade:

What can we do to make the SPA work best for business owner's? What kinds of neighbors do you want?

- More auto dealers and auto related uses such as car stereo shops, auto detail shops, things that the auto dealers do not provide.
- Prefer daytime uses
- Restaurants for employee and customers
- A gym would be an acceptable use.
- Personal Services & retail such as: dry cleaning, hair salons & office supply stores



What kinds of neighbors are undesirable?

- No massage parlors
- No 24 hour businesses
- No new motels
- No stores that sell alcohol
- No check cashing centers
- No new trailer parks

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Comments by David Wade:

Because the property values have risen, new uses will be more selective.

What are the most important things to enhance the marketability of the site?

- Visibility from the freeway is most important.
- Design standards based on visibility would be desirable, current sign ordinance regulations are restrictive.
- Would like to be allowed to use banners.
- Allow for a variety of uses.
- Improve the lighting on the street – **too dark right now.**

Do you have any issues regarding the Regulatory Process?

- The current sign ordinance is too restrictive.
- Review and Approval process for new construction is unclear; lack of communication between county departments has caused problems in the past.
- It took one ABS member 1½ years to get some property rezoned.
- One ABS member had to go through Environmental Review twice because the property was rezoned.
- Flexibility is important.
- Goal is response to these comments would be to make getting discretionary approvals easier.
- Note that the SPA ordinance addresses land use issues and not building code issues.

Do you have any operational issues?

- Off loading vehicles – off street is preferred. Some car dealers use street to off-load vehicles.
- Rear property security; this is a problem, cars get broken into and stolen, surveillance cameras were stolen.
- Consider vehicle parking garage for auto dealers.
- If buildings get higher, there are nice views of the golf course.

Comments from police:

- Avoid dark corners.
- Avoid large shrubs.
- Do not recommend off-street parking.

Design Concepts

Caltrans fence – 2 options for the fencing at the Caltrans boundary

- Chain link fencing, or a concrete K rail w/fencing.



Option 1

- Refurbish fence
- Minimal landscape
- Bike lane
- Continue the 3 lane section
- Center turn lanes
- More space at back of curb for landscaping or to display cars

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Comments

- 5:00 pm – backed up with traffic.
- Medians could be considered
- Morris onto Auburn-turning left there are trees (can't see) dangerous at 5 PM.
- Consistent merchandizing of cars @ various auto dealers.
- Display cars – currently a set back requirement – Could be revised
 - Jeff Clark – Right-of-Way line (where setbacks start...)
 - Maintenance – SPA, County maintenance of expanded ROW (right-of-way) would be an issue, properties would gain space. Could this be a maintenance issue w/private property owners?

Option 2

- Caltrans fence
- No change to curb line
- Can keep some existing @ sidewalk
- Sign program for Auburn visible from the freeway -need to review w/Caltrans

Comments

- Interested
- Caltrans Landscaping – proposed project on hold.
- Concerns about the safety of the Bike Lane.
- Likes the paved option #2 by the fence.

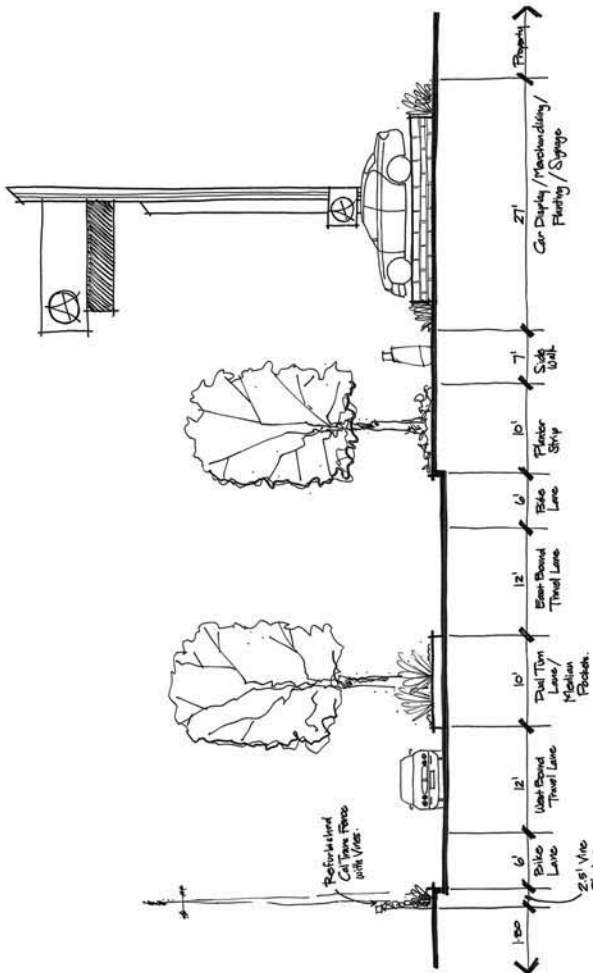
Option 3

- Have street width vary, center median with roundabouts
- “More Fun”
- Limited parking
- Advantage – could have some trees on the North Side.

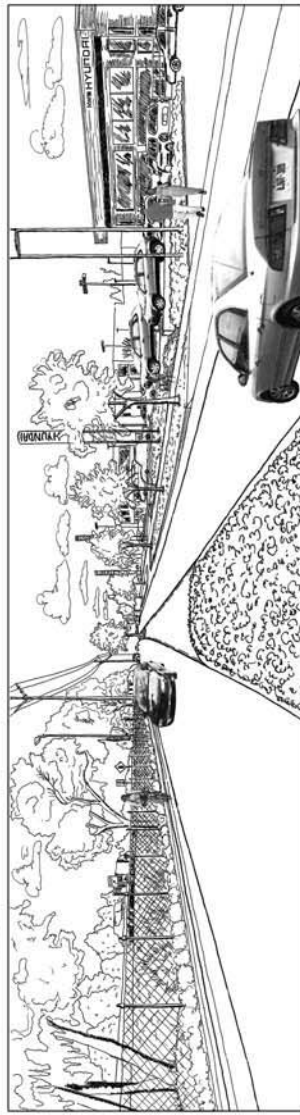
Goals

- Review – Mark up your comments on plans provided and show to Christine.
- Want Info Yes/No: What will work? What won't work?
- Don't want to launch ideas that will be detrimental. Feedback from this committee is important.
- Want to go to the first charrette w/ideas to present.
- Can mix or match ideas.
- Would like comments on all three options.

See Next page for list of attendees.



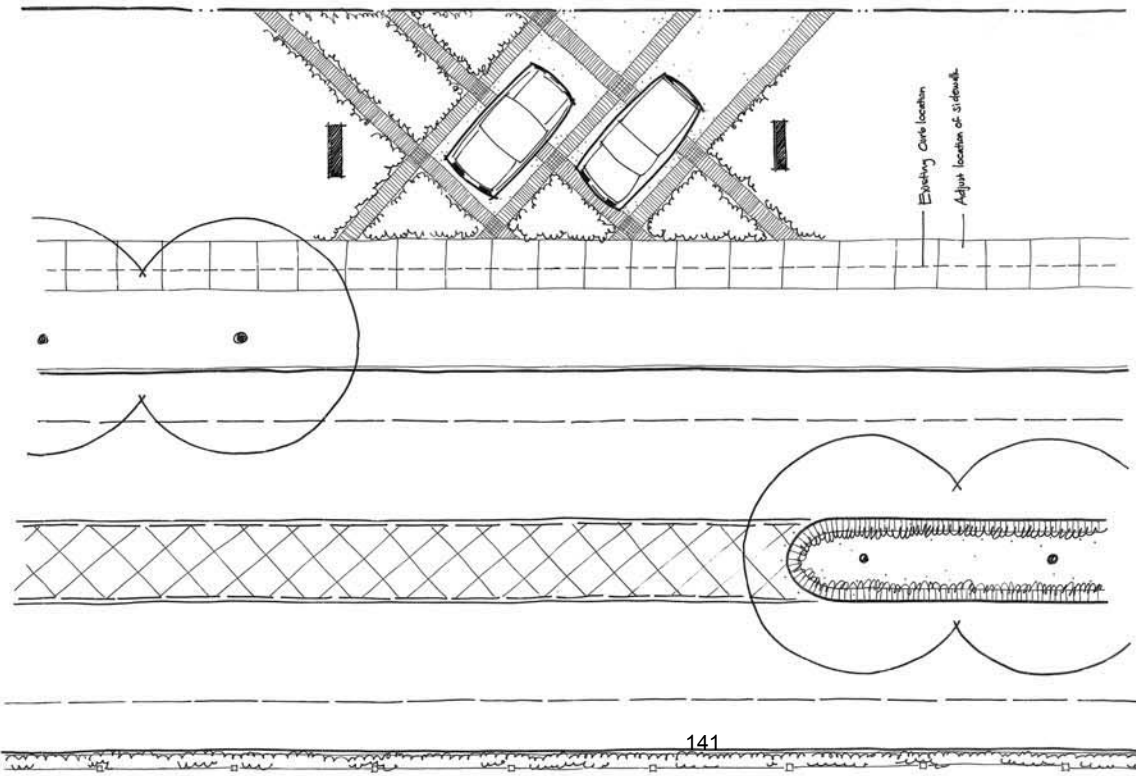
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After



Before

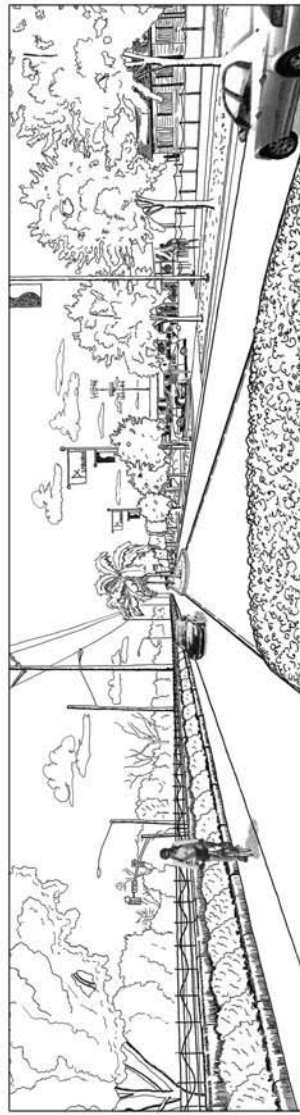
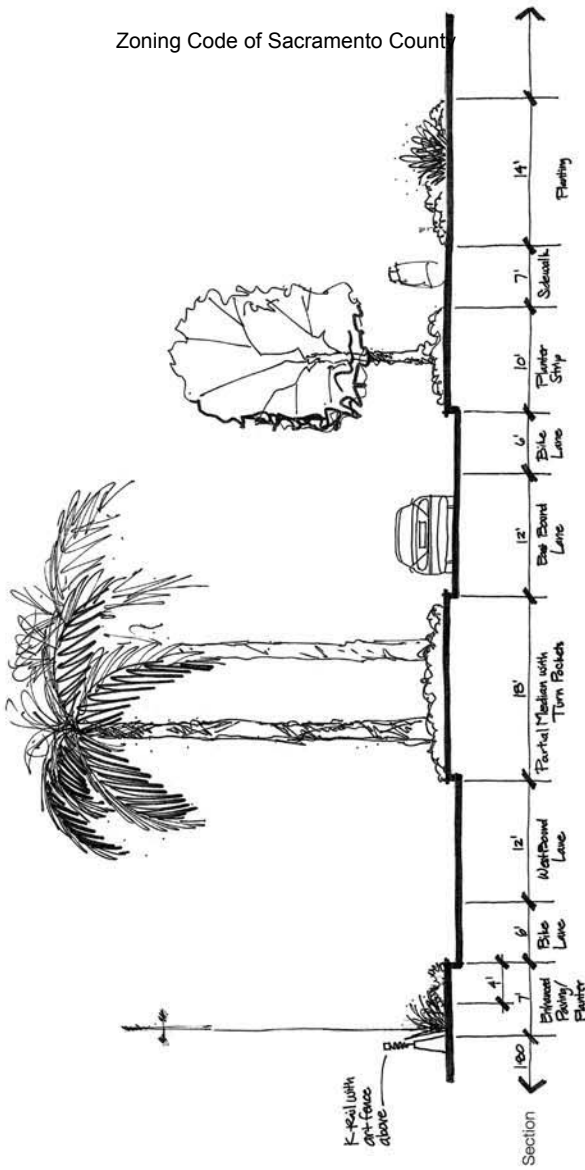


Plan

Option A

Auburn Boulevard
Special Planning Area & Streetscape Master Plan

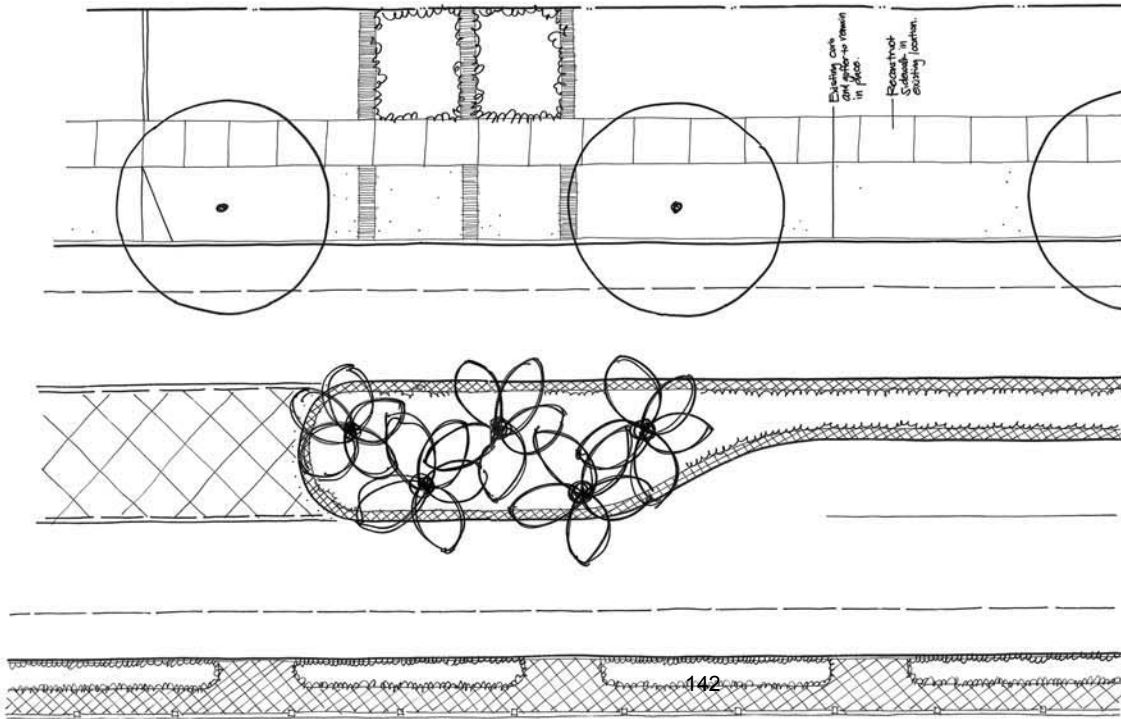




After



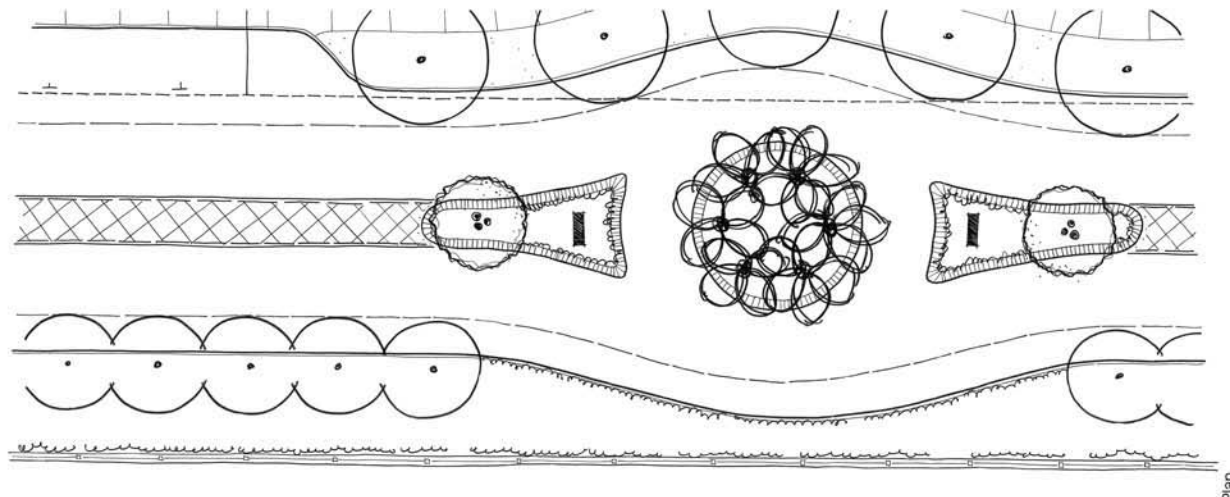
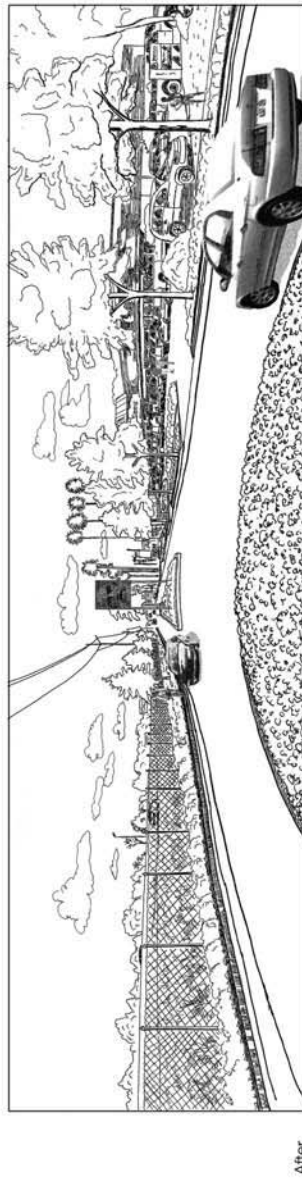
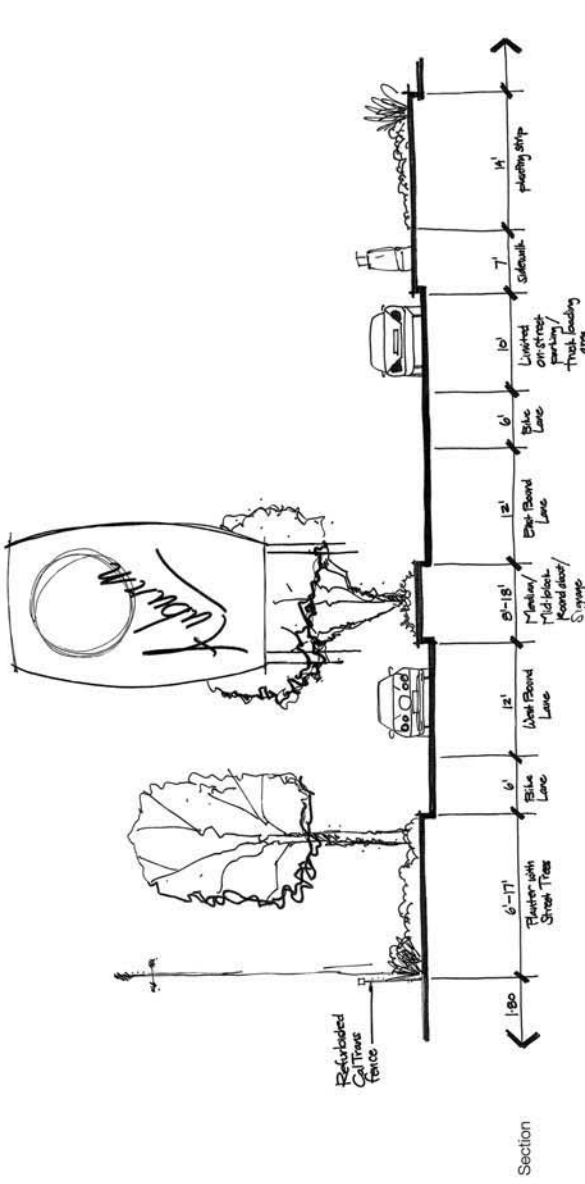
Before



Plan

Option B
Auburn Boulevard
Special Planning Area & Streetscape Master Plan





Option C
Auburn Boulevard
Special Planning Area & Streetscape Master Plan

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Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Technical Advisory Committee Meeting
January 18, 2005

Agenda

Welcome

Minutes review / Contact List

Public Notification

Discussion

- Options
 - City/CalTrans property
 - Maintenance
 - Roadway Function
 - LandUse
- Q & A

Adjourn



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) 7 Streetscape MP
TAC Meeting
Project Number: 53505.02
Meeting Date: January 18, 2006
Meeting Time: 1pm
Attending:
Recorded By: John Nicolaus

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Summary of Meeting:

1. Christine distributed agenda minutes, contact list, makes introductions
2. Christine announced that 2/9, date for next charette, 6-8 pm @ Clarion, shared newsletter w/TAC.
3. Advertisement placed in various community publications, 1500 notices sent to residents
4. Invited TAC to stakeholder meeting. Each department should have a representative @ stakeholder meeting.
5. Christine described the charette process to meeting participants. The idea is not to give charette participants a blank slate, but rather a choice of options to react to.
6. The #1 concern for stakeholders is visibility. Trees need to be spaced to not block visibility. The idea of landscape adjacent to the freeway was preferred. They were ok with having no landscaping as well.
7. David Wade suggested having a very clean look that is street sweeper friendly. Any banners would need to be at least 25' or higher. Signs and monuments were not preferred in medians.
8. Christine thinks we should share some medians ideas w/stakeholders.
9. Need to discuss with stakeholders the idea of reciprocal driveways; these may be an option.
10. Dealers would like not to off-load cars in medians.
11. Hector Barron is contact@ City of Sacramento
12. Basic Question:
 - a. How is this project improving my neighborhood?
 - b. Freeway orientation: don't downplay; try to enhance as much as possible.
 - c. It's the front porch of Arden Arcade and the neighborhood.
 - d. Neighborhood oriented service will not be located here
 - e. Make contemporary & viable.
13. Ask public members
 - a. What can you live with?
 - b. What is absolutely incompatible? Light spill onto residential parcels, traffic, noise, food odors (nuisance related) dumping, trailer parks?
 - c. Parallel parking on street is not preferred by stakeholders but will be allowed if done safely. Slow cars by having on-street parking? Need to revisit this.

MEETING MINUTES Continued

It was decided not to talk about parallel parking in discussions with the community; Conflicts were cited: RT, vehicle to vehicle, bikes. Bikes accommodated but not encouraged. Bike lane not currently striped.

Conceptual plans will be shared with the County bike committee.

Review of Plan Vignettes by Christine:

Stakeholders like palm trees

80' ROW lets us be flexible in design approach.

No impacts anticipated in Caltrans ROW

*Remember: this is a plan for the future. It all won't happen at one time.

David Wade suggests writing a program for what the street should do:

- a. Local parallel capacity to freeway when it's busy
- b. Auburn Boulevard can be just two lanes wide
- c. Think about commute traffic
- d. Landowners get together and pay for various improvements?

Program for Auburn Boulevard:

- a. Capacity – cars, peds & bikes
- b. Enhance appearance
- c. Gateway to Arden Arcade
- d. Create a clear difference between Auburn and Business 80
- e. Underground utilities—indicate this vision in the Plan
- f. Need 42' outside curb diameter for u-turns
- g. U-turn pockets are not a speed control devices, just u-turn locations
- h. County will maintain street sides if they are in a special financing District

Program for Parcels

- a. Relates to:
- b. Neighbors
- c. New and appropriate land uses
- d. Appropriate Land Use: commit to car sales.

Mike Penrose: Have him attend Caltrans, City and Community meetings. HLA will notify him so that he can attend. Mike also has input on funding and funding sources.

SHRA staff to send memo clarifying March TAC/ABS Meeting

zbd:JN

Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Auburn Boulevard Stakeholders Meeting
February 2, 2006
1:00pm

Agenda

Welcome / Introductions

Review of Minutes from October 27th meeting

Charrette Overview

Charrette Agenda
Expectations
Role of Stakeholders and Participants

Streetscape Master Plan & Special Planning Area

Opportunities and Constraints
Idea Board
Options

Adjourn



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) 7 Streetscape Master Plan
Auburn Boulevard Stakeholders (ABS) 3rd Meeting

Project Number: 53505.04

Meeting Date: February 02, 2006

Meeting Time: 1:00 PM

Attending: See sign in sheet

Recorded By: Terry Kinsler

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Summary of Meeting:

Introduction

Review of minutes from Meeting #2

Items on the minutes are being followed up.

Stakeholder objectives leading to charrette:

- Members are to help distribute flyers to others business owners, etc whom may or may not be affected by the Auburn Blvd Streetscape and ReZoning Project.
- Recruit and spread word of upcoming charrette on 02.09.06

Overview of 02.09.06 Charrette

- Introduction
- HLA will discuss presentation Boards (Opportunities and Constraints, Image Boards, Option Boards)
- Roundtable Discussions
 - Focus needs to be directed towards streetscape opportunities
 - Integration of stakeholders, public, agency members, and design team
 - Representative (preferably member of public) to discuss the table's ideas to the greater group

Opportunities and Constraints:

- The four boards showcase the current conditions of the interested stretch of Auburn Blvd.
- Boards should show the current setbacks (ROW) currently in place along Auburn Blvd.

Considerations for Design:

- Addition of businesses within project boundaries will add to the already troublesome number of employees. How will parking be accommodated within project?
- Street capacity
 - Street width, type, and number of lanes must be able to accommodate the additional traffic seeking Auburn Blvd

CalTrans

- No plans for off ramp improvements are scheduled within the next 20 years
- Accident and volume data can be provided reflecting the traffic concerning Auburn Blvd.



- Off ramp considerations
 - The need to slow vehicles down that enter Auburn Blvd via off ramp is important.
 - Off ramp traffic increases potential hazards.

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Roundtable Hot Topics:

- Addition of master plan, showcasing the long term vision of Auburn Blvd.
- Show easement setback of electrical power lines

Zoning Ordinance:

- Maintenance:
 - Who will provide maintenance care to street, city of county?
 - Should a specified level of lot maintenance and care affect business owner upkeep?
 - Is tax increment available for maintenance care? – Currently, no.
- Utility expense constraints should be posed to favor new development, rather than deter as it currently does.
- Planning codes will reflect the consensual wants and needs to create a linked district rather than the present, sparse Auburn Blvd environment.

Visual Image Boards:

- To inspire and help to spark creativity amongst those attending charrette.

Option Board – A:

- Car display platform may not be allowed due to corporate specifications.
- Corporate specifications document all aspects of car showcase displays including but not limited to size, material, etc.
- Obtaining the most current display specifications from dealerships and the requirements per display per auto mall. Display showcases may have ability to be modified to fit required needs.

Option Board – B:

- Visual impact is of high importance as not to disrupt view of signage and business store front, not only from freeway but also Auburn Blvd.
- Contains an artistic overlay above K-rail (jersey barrier).
- Consistent visual element (flag, symbol, etc) will be distributed along Auburn Blvd, helping to unify itself as a destination.
- Obtaining all elevations of surrounding areas will better allow consideration of safety issues.
- Provide visual deterrence from driving within dual left-turn lane.
- What impact do trees and shrubbery located in the CalTrans ROW have upon safety and appearance along Auburn Blvd?

Option Board – C:

- Landscaping benefactors affecting uninterrupted vision is high priority.
- Applies a pseudo-roundabout to deal with traffic issues as well as to disrupt a continuous left turn lane median

See Next page for list of attendees.

Issues

① Lack of uniform character/low curb appeal



Goals

Create a uniform sense of place

Zoning Code of Sacramento County

② Lack of available off street parking



Create an ordinance that allows flexibility in parking requirements

③ Awkward lot sizes and configurations



Allow flexibility for shared driveways and the ability to build up



Issues

④ Established existing commercial business infrastructure



Goals
 Maintain and enhance valuable businesses
 Zoning Code of Sacramento County

⑤ Growing interest in destination business



Improve the business climate for destination business with the Special Planning Area Ordinance

⑥ Differing levels of visibility from the freeway



Create better visibility from all directions of traffic



Issues

Goals

⑦ Unkempt/unloved appearance



Increase appearance and level of maintenance across the entire Boulevard

Zoning Code of Sacramento County

⑧ Unsafe/poorly designed access from freeway and unsafe traffic speeds along Auburn Boulevard



Use traffic calming devices and other design features to transition between freeway and surface streets and slow vehicle speed

⑨ Poorly coordinated on street/off street lighting design



Add on street lighting for consistent lighting levels
Create an ordinance that addresses on street lighting for the design of lighting levels



Issues

10 Oversized lane widths and number of lanes



Goals

Decrease lane widths to County standard
Reconfigure lanes to 3 lane section throughout

Zoning Code of Sacramento County

11 Change happens too slowly

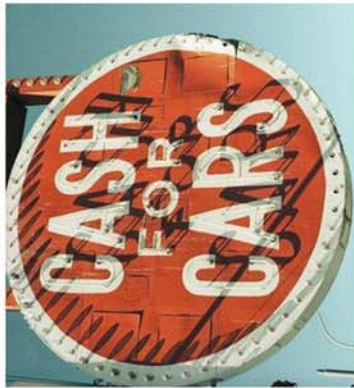


Create an ordinance that allows flexibility for various land uses without having to go through a lengthy County review process

12 Distracting overhead powerlines



Underground power where applicable
Create a different focal point



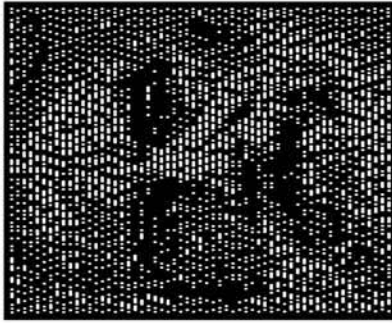
Cars
Auburn Boulevard
Special Planning Area & Streetscape Master Plan



Edison



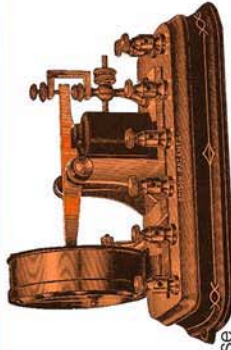
Bell



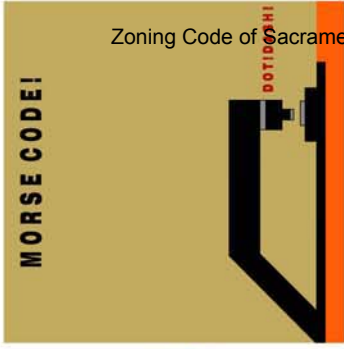
Morse



Howe



Morse

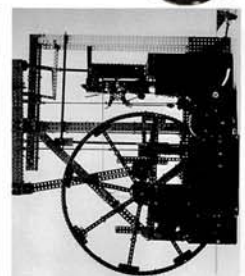


Morse

Zoning Code of Sacramento County

James Watt's 1788 Rotative Beam Engine
CANADIAN SPECIAL MODEL NUMBER 12

By Don Richmond



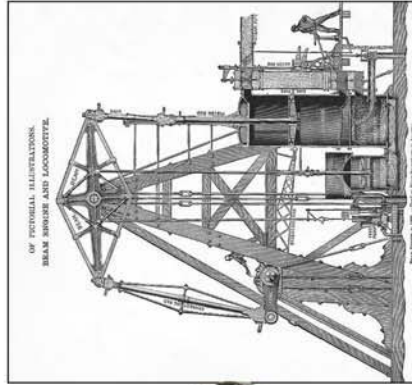
Introduction
One of a number of steam engines produced by James Watt, the 1788 "rotative beam" engine is based on the same principle as the other rotative beam engines. The rotative beam engine is a type of steam engine which produces rotary motion from the expansion of steam. It is a type of steam engine which produces rotary motion from the expansion of steam. It is a type of steam engine which produces rotary motion from the expansion of steam.

THE CANADIAN MECHANOMAN'S NEWSLETTER

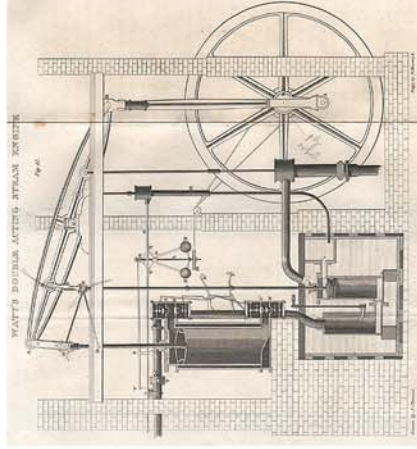
Watt



Bell



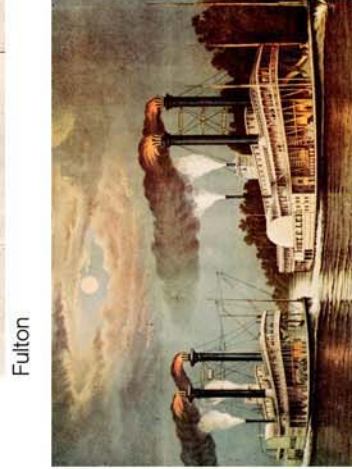
Watt



Fulton



Morse



Fulton

Bell



Edison

Bell



Bell

Auburn Boulevard Special Planning Area & Streetscape Master Plan

County of Sacramento



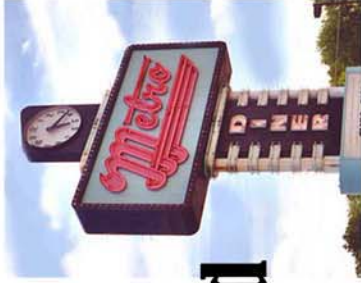
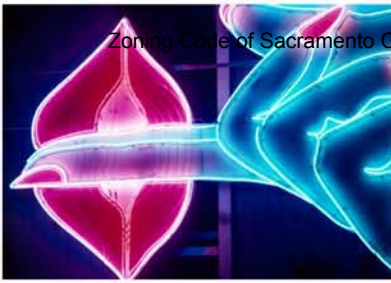
Chapter 9
Article 1

February 9th, 2006



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February 9th, 2012



Auburn Boulevard

Auburn Boulevard



AUBURN BOULEVARD



Auburn Boulevard



Auburn Boulevard



Signs & Type Auburn Boulevard

Special Planning Area & Streetscape Master Plan

The HLA Group Landscape Architects & Planners, Inc.
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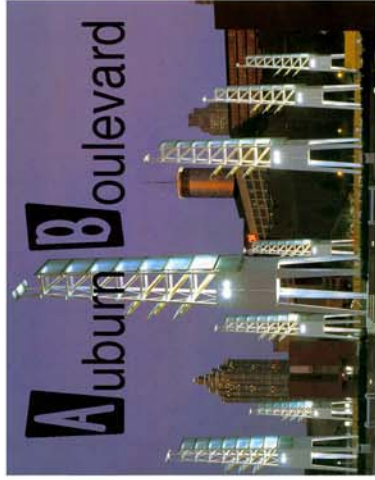


Code of Sacramento



Chapter 9
Article 1

February 9th



Auburn Boulevard



AUBURN BOULEVARD



Auburn Boulevard



Signs & Type II Auburn Boulevard

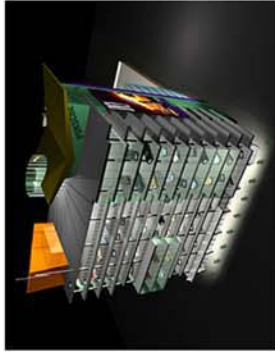
Special Planning Area & Streetscape Master Plan

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Zoning Code of Sacramento County



Architecture & Streetscapes

Auburn Boulevard
Special Planning Area & Streetscape Master Plan



Address Legend	
1	Sacramento Joy 2052 Auburn Blvd
2	Auburn Square Senior Residence 2060 Auburn Blvd
3	Oakhaven Mobile Home Park 2150 Auburn Blvd
4	Sands Motel 2160 Auburn Blvd
5	2200 Auburn Blvd Vacant
6	SHRA 2228 Auburn Blvd
7	Hampton Inn 2230 Auburn Blvd
8	Capitol Billiard SVC 2234 Auburn Blvd
9	Maita Oldsmobile Body Shop 2264 Auburn Blvd
10	John P. Livoni, MD 2288 Auburn Blvd



Zoning Legend			
Yellow	RM1 Mobile Home Park	Light Red	GC General Commercial
Orange	LC Limited Commercial	Red	C2 Commercial (City)
Light Pink	TC Travel Commercial	Dark Red	AC Auto Commercial



February 9th, 2008

Site Information/Zoning

Auburn Boulevard
Special Planning Area & Streetscape Master Plan



Address Legend

- 11 Ladi Senior Home
2300 Auburn Blvd.
- 12 Casa Grande Mobile Village
2312 Auburn Blvd.
- 13 American Bedroom & Mattress Display
2320 Auburn Blvd.
Continental Display
2324 Auburn Blvd.
American Furniture Outlet
2334 Auburn Blvd.
SDS
2326-3 Auburn Blvd.
- 14 Niello Audi
2348 Auburn Blvd.
- 15 Automatik
2400 Auburn Blvd.
- 16 Maita Subaru
2410 Auburn Blvd.
- 17 Maita Hyundai
2436 Auburn Blvd.
- 18 New Directions Graphic Apparel
2328-2 Auburn Blvd.
Triton Insurance of California
2332 Auburn Blvd.
- 19 Maita Toyota of Sacramento
2500 Auburn Blvd.
The Credit Land
2538 Auburn Blvd.
- 20 Primetime Auto
2540 Auburn Blvd.
- 21 916 Cars
3501 Fulton Ave.

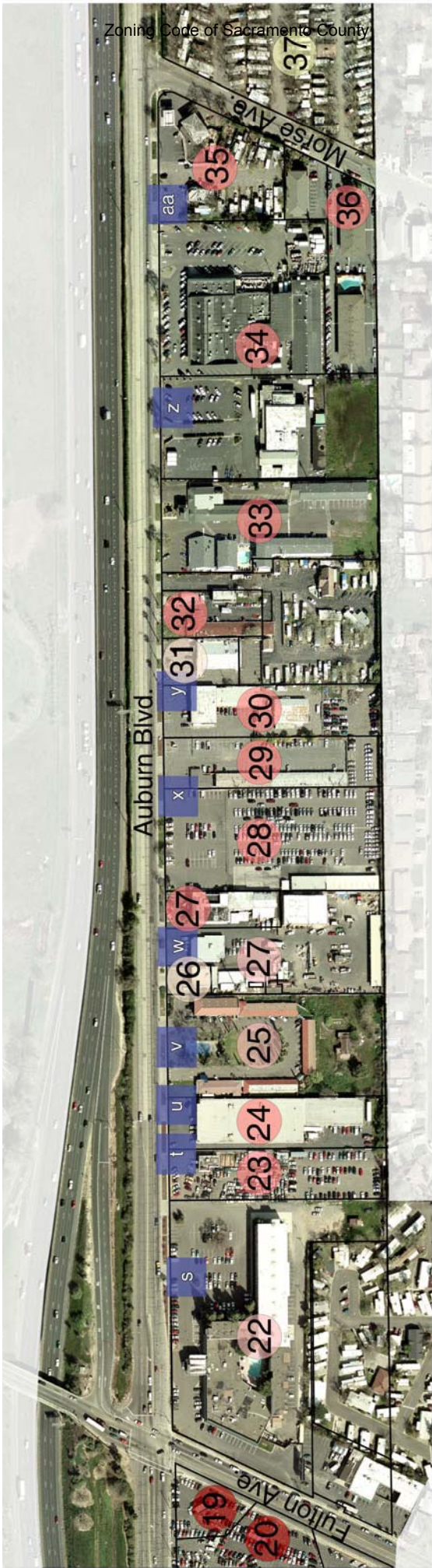
Zoning Legend

- RM1 Mobile Home Park
- LC Limited Commercial
- TC Travel Commercial
- GC General Commercial
- C2 Commercial (City)
- AC Auto Commercial

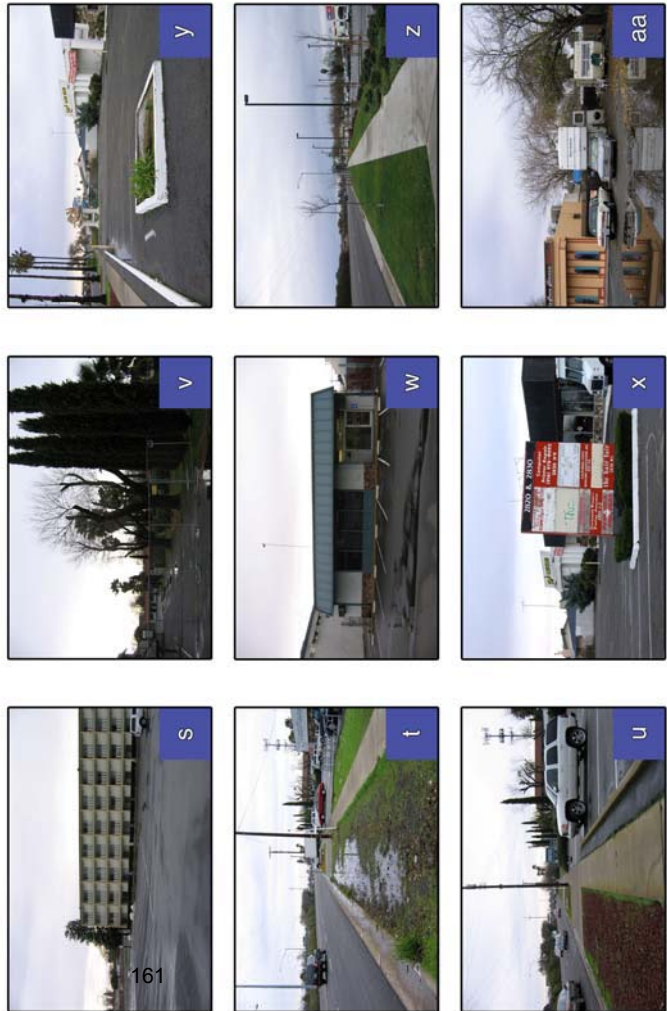
Site Information/Zoning

Auburn Boulevard
Special Planning Area & Streetscape Master Plan





Address Legend	
22	Clarion Hotel/ Enterprise Rent a Car 2600 Auburn Blvd
23	Automatik/Bernstein Automotive 2690 Auburn Blvd
24	Hobrecht Lighting Co 2690 Auburn Blvd
25	Forty Niner Motor Lodge 2730 Auburn Blvd
26	Capitol Foil & Embrossing 2734 Auburn Blvd
27	Skip's Music/Corporate 2736 Auburn Blvd
28	Maita Car Lot 2804 Auburn Blvd
29	Computer & Printer Repair Center Meest Shipping 2820 #1 Auburn Blvd
30	Barstools & Dinette Outlet 2830 Auburn Blvd
31	Salazar's Dance Studio 2840 Auburn Blvd
32	Tradewinds Motel/RV Park 2844/48 Auburn Blvd
33	Johannes Restaurant 2906 Auburn Blvd Econo Lodge 2912 Auburn Blvd
34	Deseret Industries 3000 Auburn Blvd
35	Convenience Food Store 3040 Auburn Blvd La Rosa Mexican Restaurant 3032 Auburn Blvd Auto Pawn 3060 Auburn Blvd
36	Apartments 3709/3713 Morse Ave.



Zoning Legend	
RM1	Mobile Home Park
LC	Limited Commercial
TC	Travel Commercial
GC	General Commercial
C2	Commercial (City)
AC	Auto Commercial

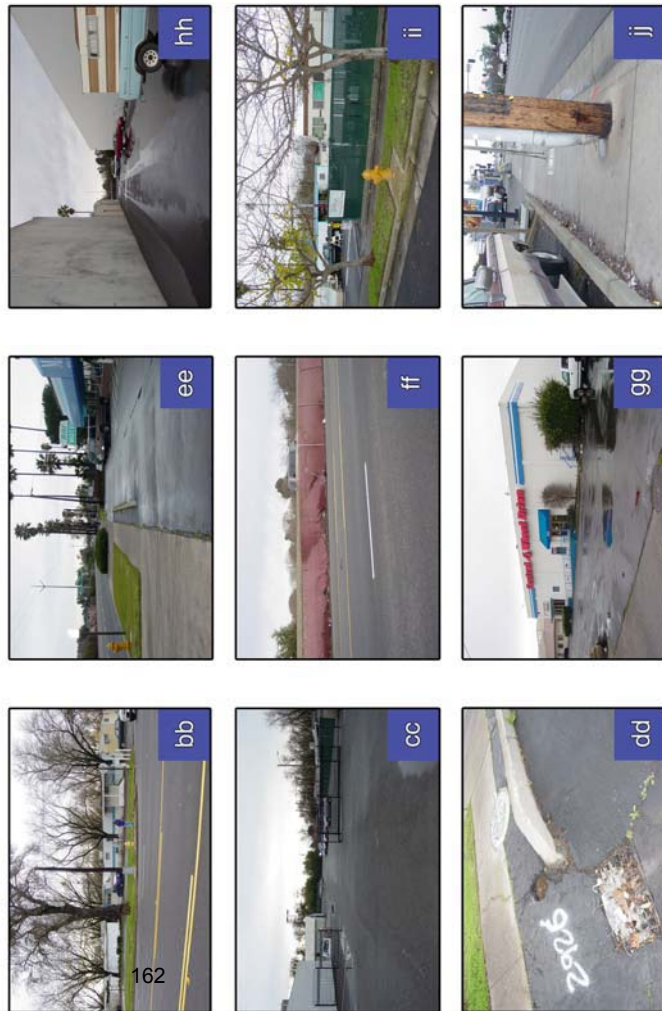
Site Information/Zoning

Auburn Boulevard
Special Planning Area & Streetscape Master Plan





Address Legend	
37	Town & Country Trailer Park 3700 Morse Ave
38	Maitia Auto Group 3124 Auburn Blvd
39	ABS Auto Auctions 3128 Auburn Blvd
40	Bullard Construction 3136 Auburn Blvd
41	McCreery's Fine Furniture 3140 Auburn Blvd
42	Surf Motel 3204 Auburn Blvd
43	Golden Tee Motel/Restaurant 3210 Auburn Blvd
44	Tuscaney Furnishings 3230 Auburn Blvd
45	Appliance Parts 3240 Auburn Blvd
46	Central 4 Wheel Drive 3248 Auburn Blvd
47	Glass Doctor 3300 Auburn Blvd
48	J.R. David Stone Gallery 3320 Auburn Blvd
49	H&B Towing 3322 Auburn Blvd
50	Caravan Village Mobile Home 3350 Auburn Blvd
51	Production Framing, Inc. 3412 Auburn Blvd
52	Import Specialists 3416 Auburn Blvd
53	Sacramento Cookie Factory 3428 Auburn Blvd
54	Arco AM/PM 3921 Watt Ave.
55	Apartments 3911/3901 Watt Ave.



Zoning Legend			
Yellow	RM1 Mobile Home Park	Light Red	GC General Commercial
Light Orange	LC Limited Commercial	Red	C2 Commercial (City)
Light Pink	TC Travel Commercial	Dark Red	AC Auto Commercial

Site Information/Zoning

Auburn Boulevard
Special Planning Area & Streetscape Master Plan



Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Charrette #1
February 9, 2006
6:00pm

Agenda

Welcome

Introductions / Announcements

Project Background / Schedule

What is a Master Plan / Special Planning Area Ordinance?

Project Detail

Site Information
Opportunities and Constraints
Vision
Master Plan / LandUse Options

Roundtable Discussions

Summary

Adjourn

Thank you for attending tonight's meeting.
Our Next Meeting will be March 23 at 6pm.



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CHARRETTE MINUTES

Project Name: Auburn Boulevard SMP and SPA
Project Number: 53505.04
Meeting Date: February 9, 2006
Meeting Time: 6pm to 8pm
Recorded By: Briana Cox

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Summary of Meeting:

Welcome

Christine Anderson welcomes everybody to the workshop. She explains to the workshop attendees that Auburn Blvd. is starting to see new and increased investments and it is a good time to start talking about a vision for the future of Auburn Blvd.

Introductions / Announcements

- Christine Anderson introduces Susan Peters – County Supervisor.
- Susan Peters welcomes everybody to the workshop and expresses her enthusiasm for the night and the future of Auburn Blvd. The desire to keep the momentum going on Auburn Blvd is important during the design process.
- Christine Anderson introduces the new Community Prosecutor – Susan Nelson.
- Project staff from the City, County, SHRA, and Consultant Team were introduced.
- Christine reminds everybody of the sign-in sheet, newsletter, and contact sheet on the entrance table.

Project Background / Schedule

Christine Anderson gives an overall description and components of the project schedule. The specific actions discussed included:

- The project area begins west of Howe Ave. and ends at Watt Ave.
- SHRA Redevelopment Area
- One lot deep along the entire stretch.
- Approximately two miles long
- Very aggressive schedule
- Streetscape master plan and special area ordinance plan to be done by the end of the year.
- Need to get as much information from the public early on in order to accomplish the final goal.
- Two other workshops will be held for public input.



What is a Master Plan / Special Planning Area Ordinance?

A brief overview and description of a master plan and a special planning area ordinance were discussed. A brief outline of potential issues addressed with each is as follows:

Master Plan

- Connectivity
- Signage
- Landscape
- Auburn Boulevard's interface with Business 80
- How should Auburn Blvd. be redeveloped?
- Cost estimate of the work that lies ahead in order to realize the vision for Auburn Blvd.

Special Planning Area Ordinance

- There are a number of zoning areas along Auburn Blvd.
- Commercial zoning areas make up the majority of the zoning areas along Auburn Blvd.
- The task is recognizing that this is an area going through change
- SPO- modifies rules of zoning along Auburn Blvd. from the current
- Many questions will be raised about zoning and design

Project Details

Image Boards are used to show examples of what Auburn Blvd. could become. The image boards are explained and how they apply to Auburn Blvd.

Signs

- Set a tone along the boulevard
- Give an image or identity for a place

History

- Identifies the community and describes history of place.
- Adds a layer of richness and character
- Can draw from the history of a place and implement it into the future design.

Car Dealerships

- A big part of the areas economy
- Opportunities to use the Car Dealerships as a defining feature along Auburn Blvd.
- Opportunity to utilize views from the freeway
- Architecture of Car Dealerships can enhance the look of the area

Site Information Boards are used to show the current conditions along Auburn Blvd., as well as the current zoning. The current zoning will be important as the design process moves forward and the special planning area ordinance starts to take place.

Opportunity and Constraint Boards discuss the major issues along the Boulevard. Twelve issues and solutions are discussed.

Three design Option Boards are discussed. They're used as "the toolkit" to generate further discussion on what the public would like to see happen along Auburn Blvd.

Questions raised from the workshop participants:

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1. How many business owners are present? *Approximately 25%*
2. Will SPA override sign ordinances? *It can, but complying with Cal Trans has to be taken into consideration*
3. With regards to traffic, does the design options that show changing the lane configuration along Auburn Blvd. take into consideration the future development along Auburn Blvd.?
4. Are bike lanes taken into consideration?
5. Will Auburn Blvd. be ADA accessible?
6. There has been a lot of discussion about Auburn Blvd. east and west, but about north and south? How do you connect residents and businesses to the other side of the freeway?

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Roundtable Discussions

Table 1

- Landscaping emphasis along North side of Auburn.
- Chain link fence
- Bike lanes – inconsistent / unrealistic
- Divide bike lanes needed (if required)
- Like Option A signage
- No billboards
- No large deciduous trees blocking commercial buildings on the south side.
- Unifying element
- Cal Trans get involved though maintenance agreement
- Landscaping on business side
- On street parking is problematic – creates visual clutter
- Signage high enough to be seen from both sides of I-80 (+25)
- Don't take from property owners for landscaping buffer
- Detached sidewalk is good, but make back of walk edge
- Don't use cute imagery for inspiration
- Make Auburn Blvd. pretty
- Don't need to pay homage to inventors
- Make the new design of Auburn Blvd. timeless
- Get Irvine photos—signage examples
- More example of landscaping along one side

Questions:

- Have trailer tractor turning radii been considered with road narrowing?
- How will this be paid for?
- Will there be checks in place to make sure new improvements are not ripped up?
- What type of local transportation will be provided?
- How many examples are there with businesses on one side of an arterial street?

Table 2

- Mixed use
- Beautification needs to happen
- No medians – will hurt smaller businesses
- Leed approved building will help move through building process—help get building permit



- Signage doesn't benefit everyone
- Have Auburn connect with surrounding neighborhoods and streets
- Fulton overpass or tunnel
- Pattern and meandering will give better appearance
- Fancy signs only target certain businesses
- Should theme around surrounding streets
- Bulb outs and traffic circles will help with traffic calming

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Table 3

- Focus on some small features
- Need for some local serving businesses—ice cream, grocery, etc. Locals would use businesses
- Businesses that would serve a senior population
- Bikeway is important
- Need bus stop for convenience –north side too
- More trees in parking lots
- Separated sidewalks – pedestrian access
- Pedestrian bridges over freeway
- Provisions for maintenance
- Exhibits along boulevard

Table 4

- Keep history of street
- Create a “people place”—housing and mixed uses
- Focus on multi-story development
- Develop out – closer to Auburn Blvd.
- Bring in small shops / boutique commercial
- Visibility from traffic
- Respect existing neighbors
- No tall signs
- No medians – maybe a meandering sidewalk with benches and landscaping
- Incorporate north bus stop
- In favor of ‘Gateway’ concept as long as an actual value is achieved.

Table 5

- Address the North side of the street – once a week clean up is not enough
- Business diversity
- Market will tell who the valuable businesses are
- Stable business / get commitment on funding
- Visibility from west bound traffic is awful
- Why is Cal Trans center barrier so high?
- Businesses could financially participate in cleaning up the Cal Trans frontage
- Marconi curve to Howe – spinouts / accidents – wall would be good
- No speed bumps , No roundabouts
- Change from 4 lanes to 3 lanes. Heavy cost—make sure this doesn't create a greater problem
- Left turn in and out of businesses
- Don't block access to driveways
- K rail would be better – especially on curve
- Signage that can be seen from freeway (both sides) would be key
- If you can get height on signs that would be optimum – 30ft.



- Don't block access and visibility
- In the car business – signage is controlled by the car company
- Branding the strip is good
- Inventors – Modern interpretation of History

Table 6

- Wider pedestrian sidewalk—a walkway
- Auburn should resemble Fulton in order to keep neighborhood feel
- More bus stops
- Public transit along Auburn Boulevard—a shuttle
- Pedestrian Bridge over freeway
- Security lighting
- More roundabouts at intersection-u-turn access
- Recreation Center for the County
- “Boulevard of the Stars”
- No Palms

Summary

Table presented the key issues discussed during the roundtable.

Announcements – next workshop will be held at the Clarion on March 23 at 6pm.

Meeting adjourned by Christine Anderson

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Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Charrette #2
March 23, 2006
6:00pm

Agenda

Welcome

Introductions / Announcements

ReCap of Charrette #1

Goals

- Aesthetic
- Functional
- Planning

Design Response

- Streetscape Master Plan
- Special Planning Area Ordinance

Roundtable Discussions

Summary

Next Steps

Adjourn

Thank you for attending tonight's meeting.



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CHARRETTE MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) & Streetscape Master Plan (SPA)
Project Number: 53505.06
Meeting Date: March 23, 2006
Meeting Time: 6:00 P.M.
Attending: See sign in sheet
Recorded By: Chris Cox

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- **Introduction by John**
- **Recap of last Charrette (Powerpoint)**
- **Response to what we heard from last charrette (Powerpoint)**
- **Roundtable Discussion**
 1. Question raised about whether drawings depict proposed development
- **Group Discussion**
 1. Table #1
 - a. Trailer Park @ Morse & Auburn
 - i. Considered unsafe
 - ii. Children and other people are crossing the streets to get to the store
 - iii. Provide for a traffic signal
 - b. Landscape buffer between residential & business a plus
 - c. Bike paths are a good idea
 - d. Signs
 - i. Auburn Blvd. sign well received, but should say more than just "Auburn"
 - ii. Inventor signs were a hit as well
 2. Table #2
 - a. Street [Auburn] is very linear
 - i. Soften with landscaping @ k-rail
 - ii. Potential problems foreseen with landscape maintenance
 - iii. Redesign street so it is not such a straight shot
 - b. K-rail is a tagger's dream
 - i. What can we do to prevent vandalism?
 - c. Shared Driveways
 - i. Differentiate paving for pedestrians & autos
 - d. Sculptural Signage
 - i. This was a hit
 - ii. We need more repetitious elements
 - e. Building height seen as good and necessary
 - i. Possible lofts on top floors
 - f. Bring restaurants back (Apparently there used to be more of them)
 - g. Auburn sign needs to be more descriptive
 - i. We are not in Auburn & don't want to confuse people



- h. Can we get this presentation/information online?
- 3. Table #3
 - a. Visibility from freeway overemphasized as a design element
 - b. The design is turning it's back to nearby housing
 - c. Residential areas should be included on Auburn Blvd.
 - d. K-rail
 - i. Is there an alternative?
 - ii. Can it be custom made to look better?
 - iii. Does not look upscale
 - iv. "Upscale" has not been brought up enough
 - e. Give Auburn Blvd. the same level of treatment that Roseville, Folsom & Rancho Cordova have used. (Maybe we should show them Historic Roseville)
 - f. Monument signs on Fulton are not useful
 - g. "Underwhelmed" with the design
 - i. "We want upscale, upgraded amenities like other neighborhoods"
 - ii. More cohesive style
 - iii. Attractive lighting
 - h. Are two lanes enough?
 - i. Concern that 1 lane in each direction will not accommodate proposed developments
 - i. SPA process
 - i. Does it have enforcement to get what we want?
 - ii. It needs to be very specific
 - iii. Should not be abused, or overridden
 - j. Consistent tree type
 - i. Shady, large trees wanted
 - ii. Do not rely on palm trees
 - iii. Need to be consistent
 - k. More wanted from the county
 - i. County's opinion is that "you should be satisfied with what you get. This is good enough for you"
 - l. Pedestrian overpasses wanted over freeway
- 4. Table #4
 - a. Financing
 - i. Who is paying for all of this?
 - ii. Unknown debt is an issue
 - b. Phasing is important
 - i. There is a difference between a vision plan (now) vs. an implementation plan (which should be later)
 - ii. Disclosure is important
 - iii. It seems that the plan is much further than just a vision
 - iv. No proposals wanted at this time
 - c. Leave the street as is
 - i. We depend on trucks and they need access
 - ii. Medians will block them
 - iii. More than two lanes needed
 - iv. Just clean up the trash and the fence
 - d. Bike lane creates liability
 - i. Conflicts with bus traffic
 - ii. Highway accidents from people crossing over landscape buffer
 - e. Concerns about available ROW

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- i. Do we actually have space to implement these ideas?
 - f. Shared parking and shared driveways
 - i. Access between lots not well received
 - ii. Visitors/customers will not want to park in rear
 - iii. Police will not go onto private property, so who will monitor the area?
 - iv. Private security is not free
 - g. Pedestrian crossing at bus stop by Deseree Industries
 - i. What happens when traffic stops?
- 5. Table #5
 - a. Parcel aggregation
 - i. Depends on marketplace, unless it is done through public acquisition
 - ii. Should be the result of the private market place
 - iii. It sounds good, but how can you practically do it?
 - iv. How can this process be accelerated?
 - v. Can perks be offered to bigger frontage owners?
 - vi. Code enforcement, rather than owners sitting on property (I'm not sure what code enforcement has to do with it.)
 - b. Shared parking vs. access
 - i. Access not a good idea
 - ii. Customers can't be allowed to drive through inventory area
 - iii. Shared parking with alternate hours is a possibility
 - iv. How will SHRA handle peak times?
 - c. Medians
 - i. No medians where access will be limited
 - ii. Decorative paving is a good idea
 - iii. Medians can't happen before properties are aggregated
 - d. Monuments
 - i. Should be put far enough from street to prevent accidents
 - ii. Placing them on private property is okay
 - e. More landscaping wanted on north side of Auburn Blvd.
 - f. Speed control signals suggested along Auburn Blvd. instead of synchronized signals that are currently in use.
- 6. General Discussion
 - a. Bike lanes, or no bike lanes?
 - i. Bikeway masterplan to be updated this year.
 - ii. If bike lanes are not wanted, there should be input to this masterplan
 - iii. Adding bike lanes could be a way to get more money for improvements
 - iv. Are there safe ways to engineer the street for bike lanes at bus stops?
 - v. Where else could a bike lane go?
 - b. Are two lanes enough?
 - i. Having one lane in each direction will back up traffic when cars are turning into businesses
 - c. K-rail
 - i. Question asked about why we are using a k-rail. (Christine explained Cal Trans requirements)
 - ii. How high is the k-rail and is that difference on the Auburn Blvd. side, or the freeway side?
 - iii. Are there going to be vines growing on it?

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- iv. Does safety need to be improved for drivers on Auburn Blvd. to prevent accidents from cars on the freeway jumping the k-rail, or fence?
- v. Can there be anything done to improve visibility & safety?
- vi. Conclusion is that we need to look at traffic data
- vii. Consider talking to people on Auburn Blvd. regarding accidents (Question raised by Holbert Lighting)

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- **Wrap-up**

- 1. Explanation of plan preparation & DERA submittal

- **Additional Notes**

- 1. Note from Bill Divas (Sp?)
 - John, One of the objectives is to “enhance the portal to Arden Arcade Community” and to enhance community identity. How can this be achieved if you never disclose in design, or signage that Auburn Blvd., & Fulton Ave. for that matter, is located in the Arden Arcade Community & not in the City of Sacramento?
 - 2. Look for connections to surrounding neighborhoods
 - 3. Don't make k-rails white
 - 4. Branding is important along Auburn Blvd.



Streetscape Master Plan & Special Planning Area



Auburn Boulevard

Charrette Number Two

Agenda

- What the design team learned from Charrette Number One
 - Aesthetic Goals
 - Functional Goals
 - Planning Goals
- Design response to these issues
 - Proposed Streetscape Master Plan
 - Proposed Special Planning Area Ordinance
- Roundtable Discussion
- Summary
- Next Steps



Workshop Number One

Aesthetic Goals

Provide a unifying character



Workshop Number One

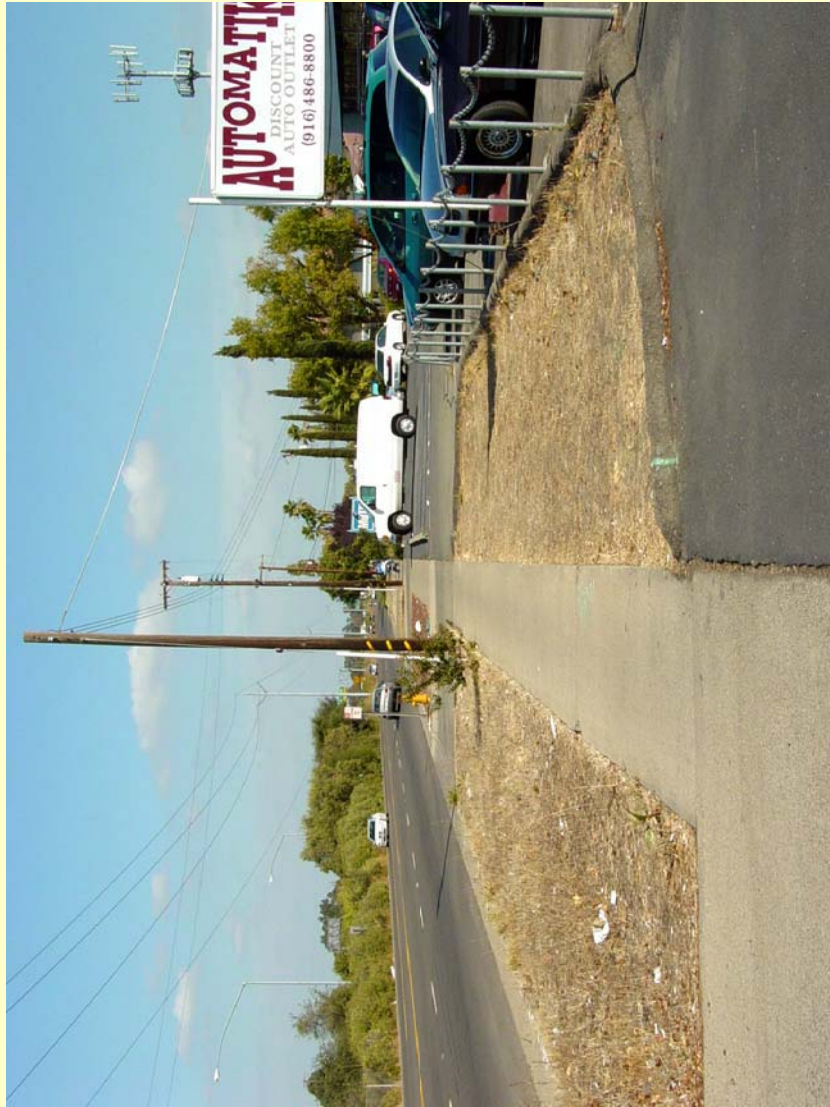
Aesthetic Goals

Promote visibility from the freeway



Workshop Number One Aesthetic Goals

Encourage consistent maintenance



Workshop Number One

Aesthetic Goals

Design ideas should:

- Be quality designs that will hold up over time
- Be compatible with Fulton Avenue
- Provide character to Auburn Boulevard
- Include large and small features
- Not be trendy



Workshop Number One

Functional Goals

Provide Accessibility and Safety

- Meet ADA requirements
- Provide access to bus stop on north side
- Provide access to driveways
- Provide bike lanes per the Bikeway Master Plan
- Provide adequate street lighting
- Continue detached widened sidewalk

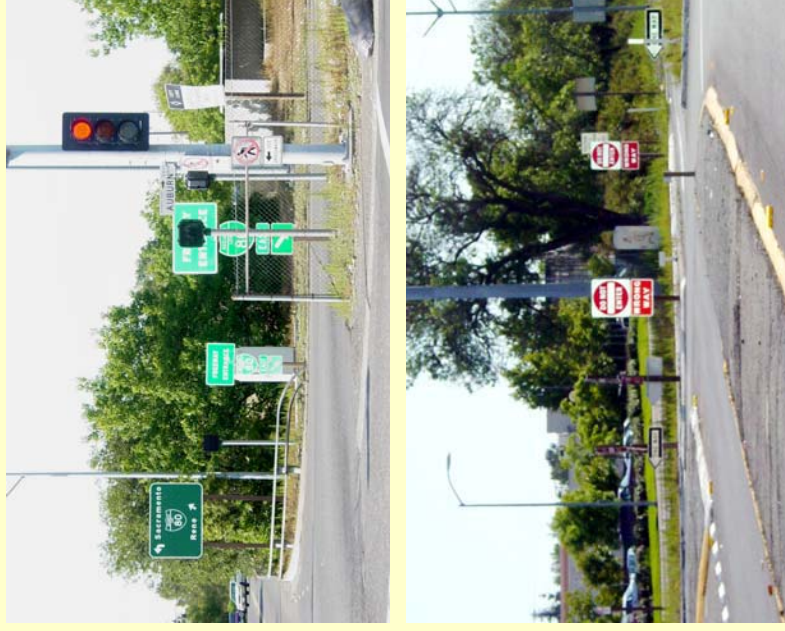


Workshop Number One

Functional Goals

Improve traffic safety and functionality

- Provide access for delivery vehicles
- Allow left turn access into properties
- Provide U-turn capability at intersections
- Slow traffic to acceptable speeds
- Make general navigation more clear





Workshop Number One

Planning Goals

Encourage variety

- Design diversity – create character
- Business diversity – plan for uses to change
- Allow flexibility



Workshop Number One

Goals of the Special Planning Area

- Enhance Retail Sales Opportunities
- Expand Community Services
- Enhance the portal to the Arden Arcade Community
- Create a sense of Place/Identity
- Enhance Traffic Safety



Workshop Number One

Planning Goals

Streamline approval process

- SPA Ordinance to provide guidance
 - Architectural design
 - Landscaping
 - Lighting
 - Parking and circulation
- Design standards will define appropriate design
- Designs compatible with the design standards will receive accelerated approval process



Workshop Number One

Planning Goals

Provide Coordination

- Coordinate public and private projects
- Avoid duplicating or conflicting projects
- Coordination between public jurisdictions



Workshop Number One

Land Use

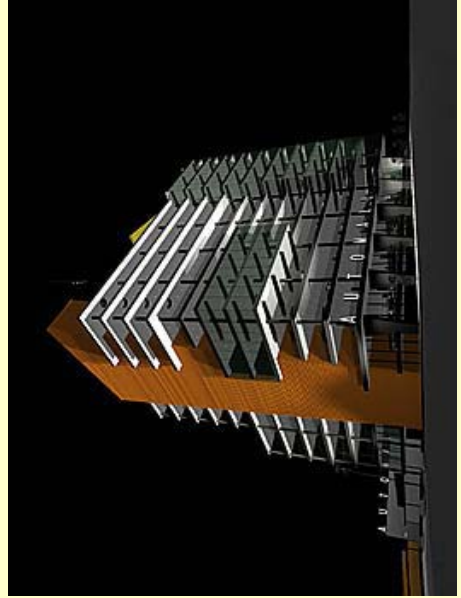
- Mix of land uses
- Compatibility among uses
- Supporting and complementary uses
- Economic viability



Design Response

Proposed Streetscape Master Plan and Special Planning Area

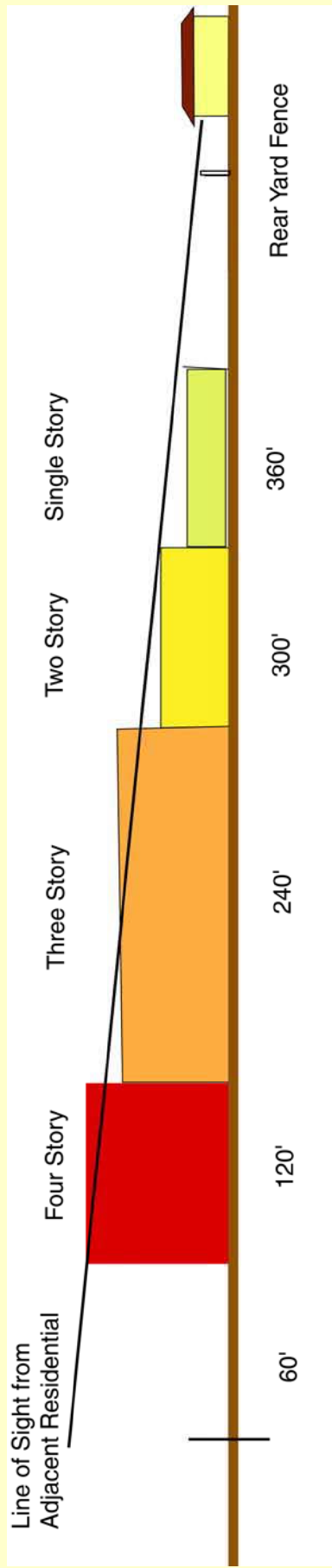
- Front setbacks deep enough to allow visible store fronts
- Higher buildings toward Auburn Blvd frontage
- Open up the center of parcels
- Share parking and driveways
- Architecture guidelines that emphasize modern style



Design Response

Proposed Streetscape Master Plan and Special Planning Area

- Allow up to 4 stories within 120 feet of street

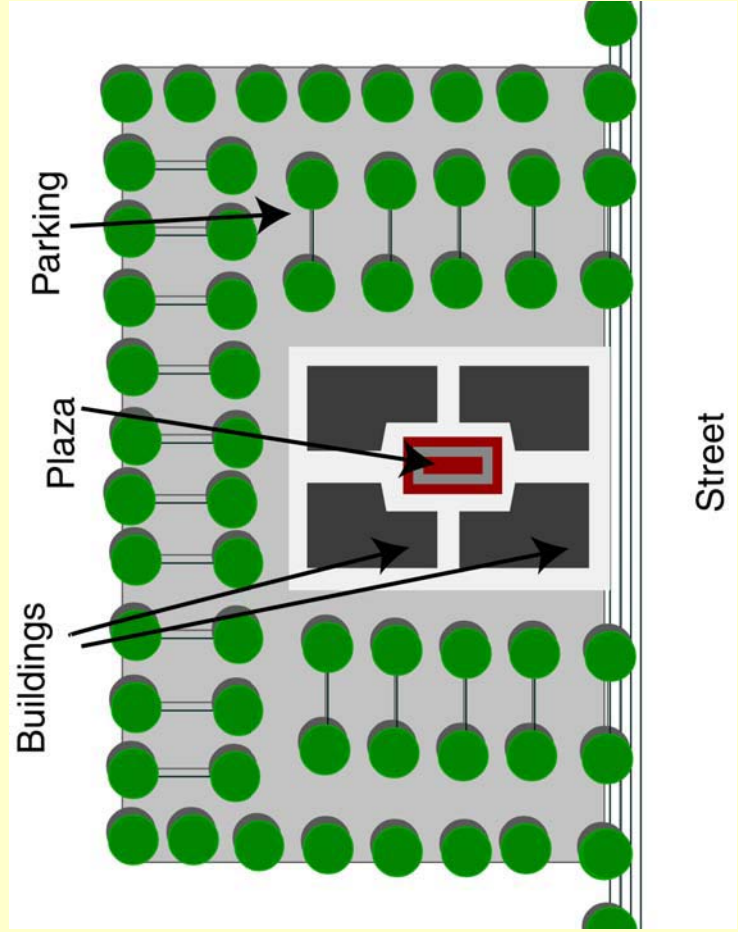




Design Response

Proposed Streetscape Master Plan and Special Planning Area

- Open up the center of parcels
- Share parking and driveways



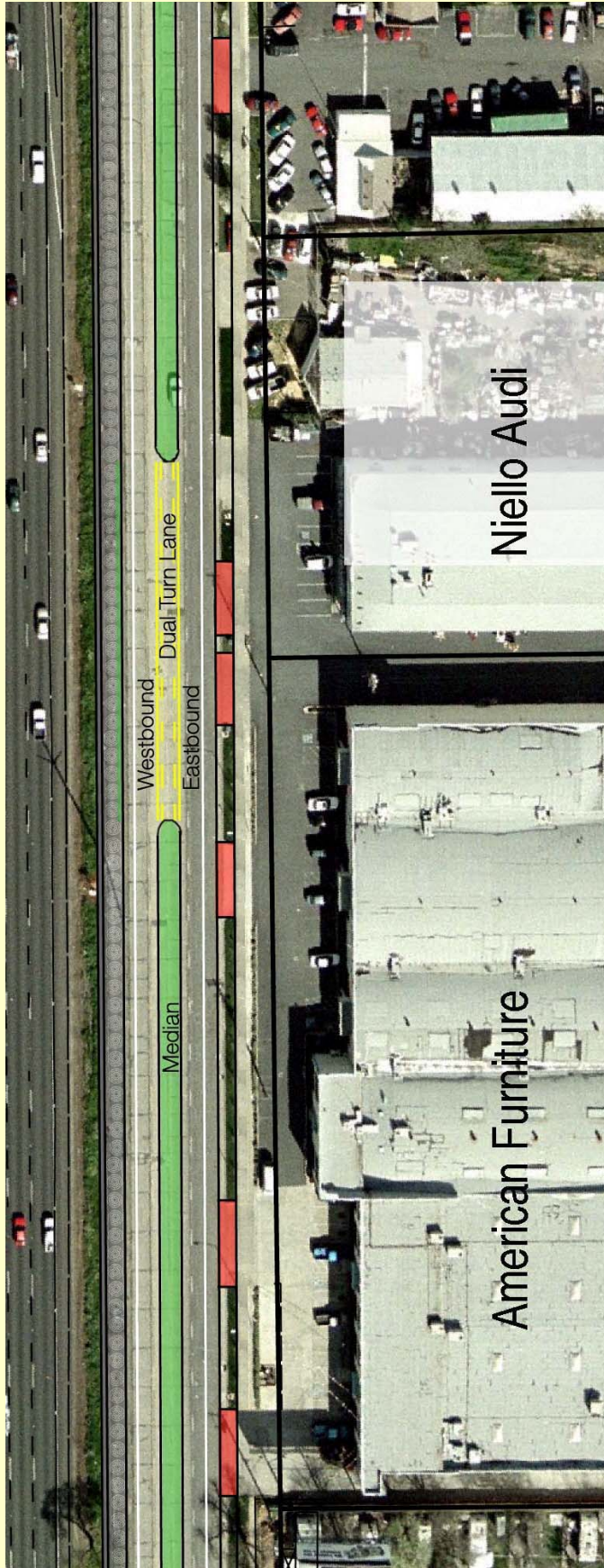
Design Response



Design Response

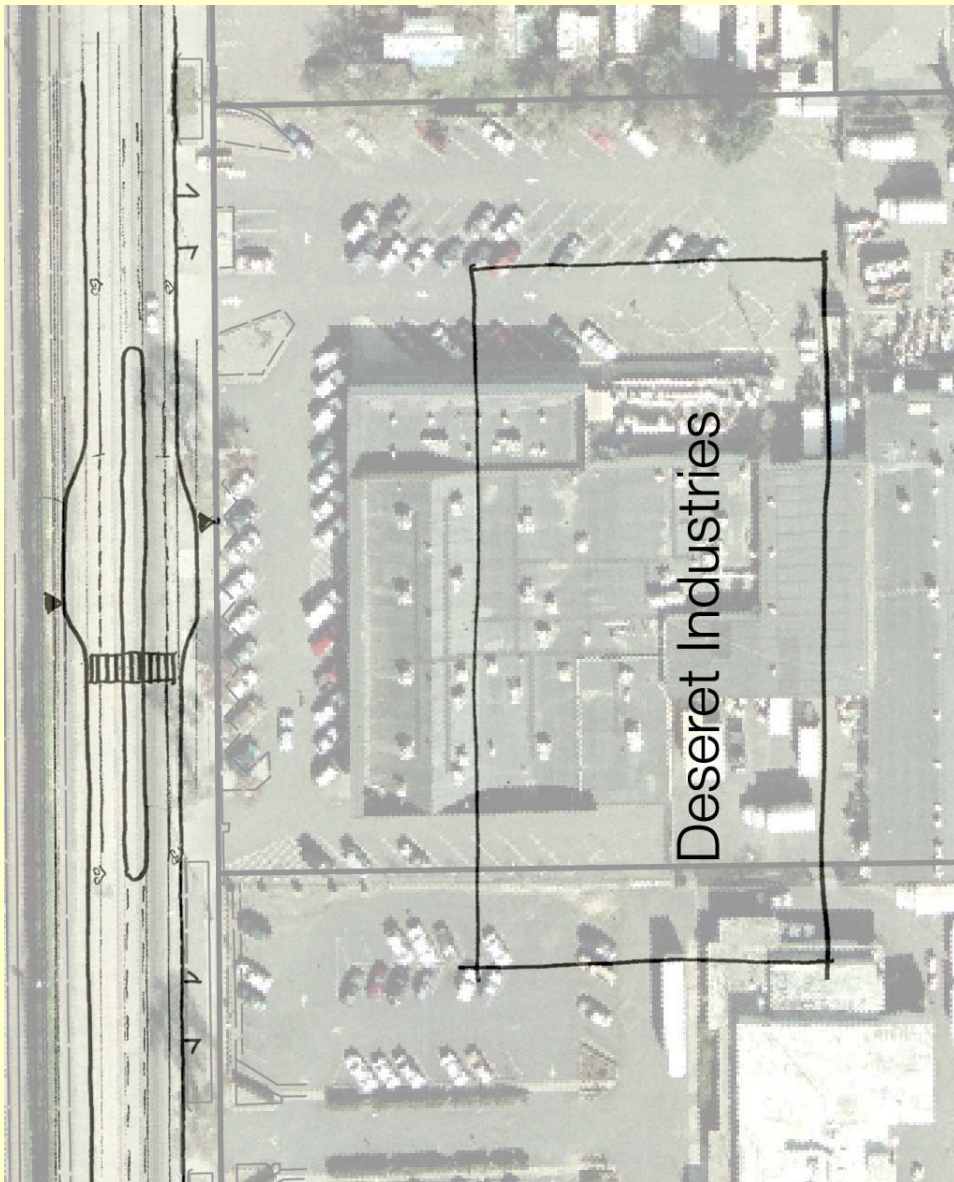


Design Response

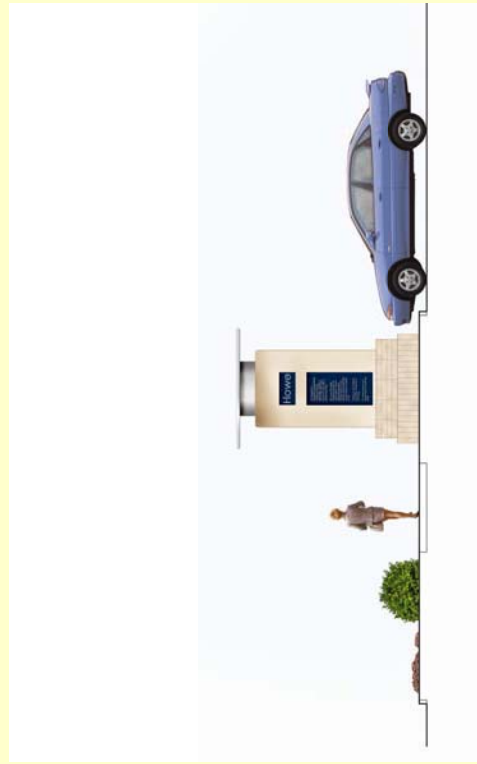




Design Response

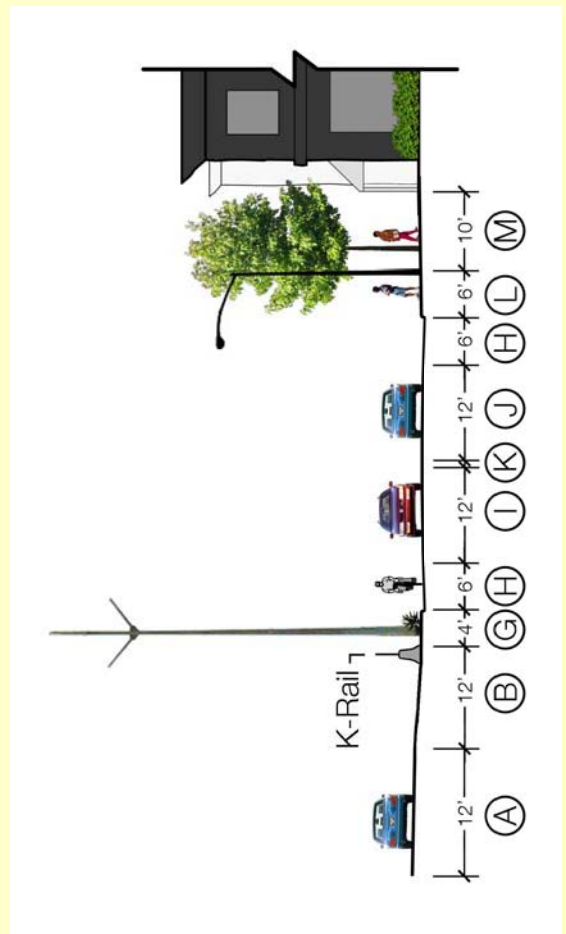
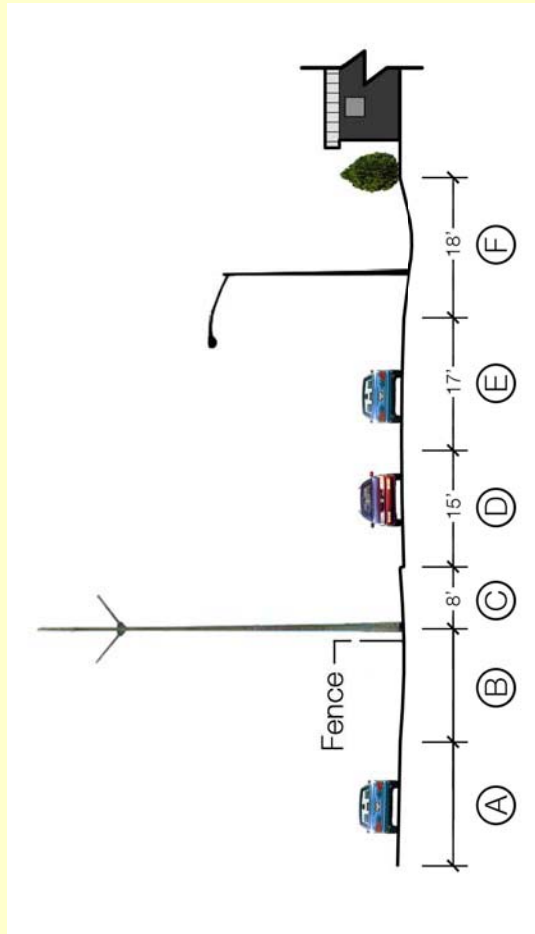


Design Response Signage



Design Response

Section A



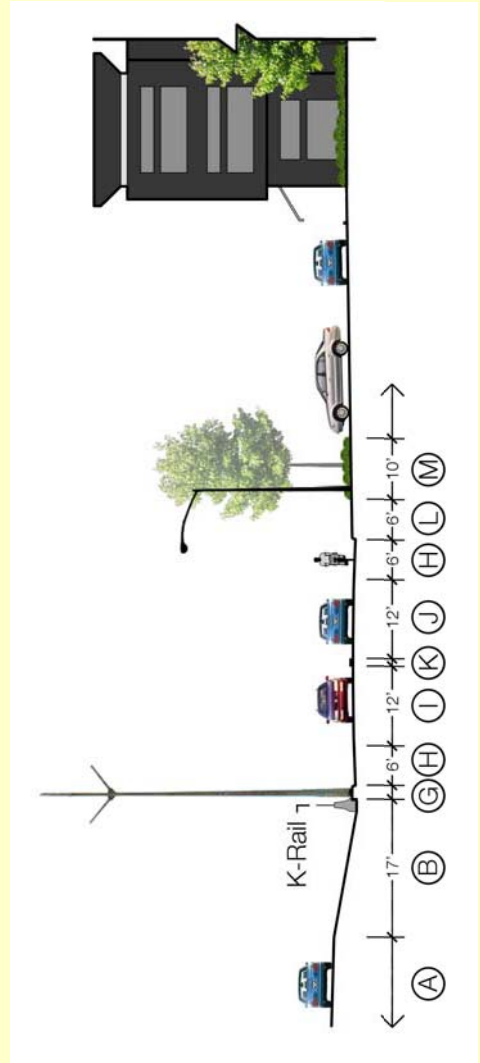
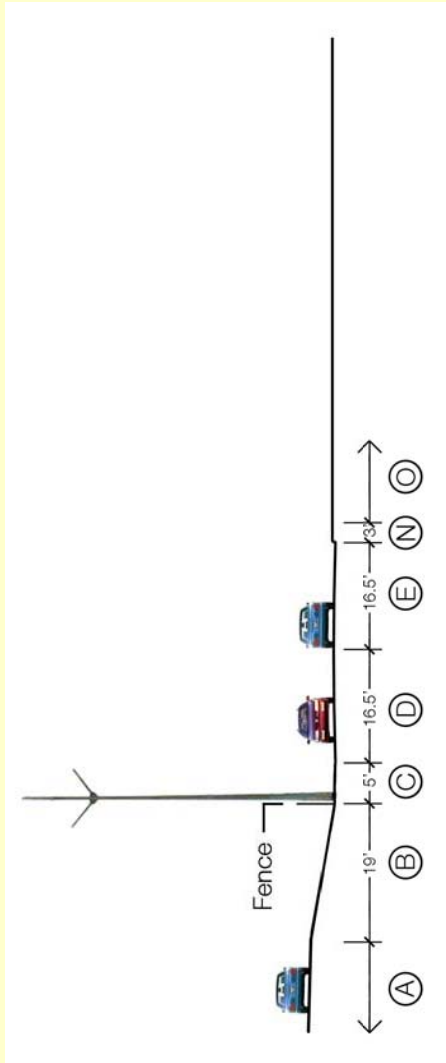
Legend

(A)	Eastbound Business 80
(B)	Highway Shoulder
(C)	Unimproved Roadside Shoulder
(D)	Westbound Lane
(E)	Eastbound Lane
(F)	Unimproved Street and Landscape Setback
(G)	Curb, Gutter and Enhanced Paving
(H)	6' Bike Lane
(I)	12' Westbound Lane
(J)	12' Eastbound Lane
(K)	Striped Divider
(L)	6' Sidewalk
(M)	10' Minimum Landscape Setback
(N)	3' Sidewalk
(O)	Vacant Parcel
(P)	2 Westbound Lanes
(Q)	2 Eastbound Lanes
(R)	10' Landscape
(S)	11' Median / Dual Turn Lane
(T)	Northbound Lane
(U)	Southbound Lane
(V)	12' Northbound Lane
(W)	12' Southbound Lane



Design Response

Section B



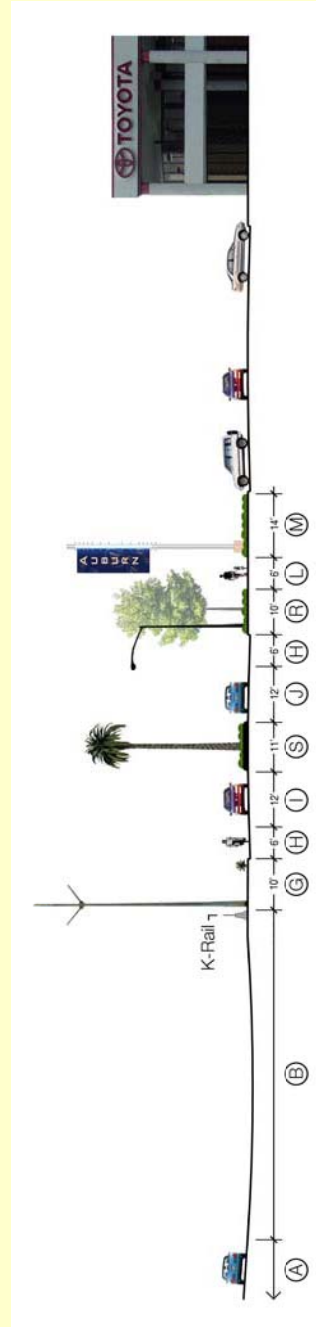
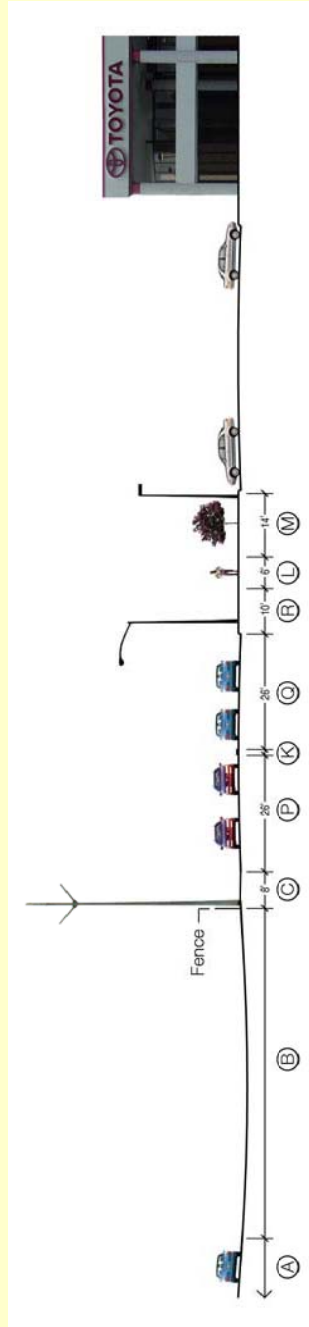
Legend

(A)	Eastbound Business 80
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(S)	11' Median / Dual Turn Lane
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(U)	Southbound Lane
(V)	12' Northbound Lane
(W)	12' Southbound Lane



Design Response

Section C



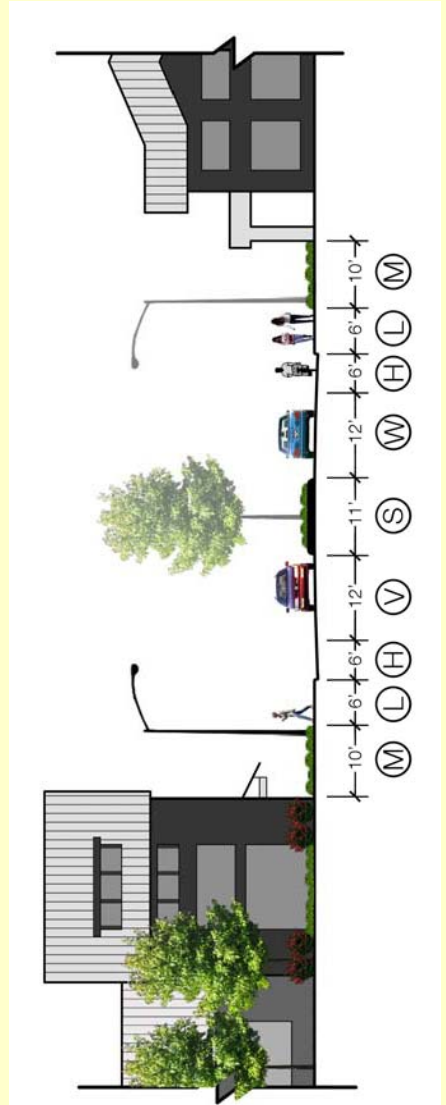
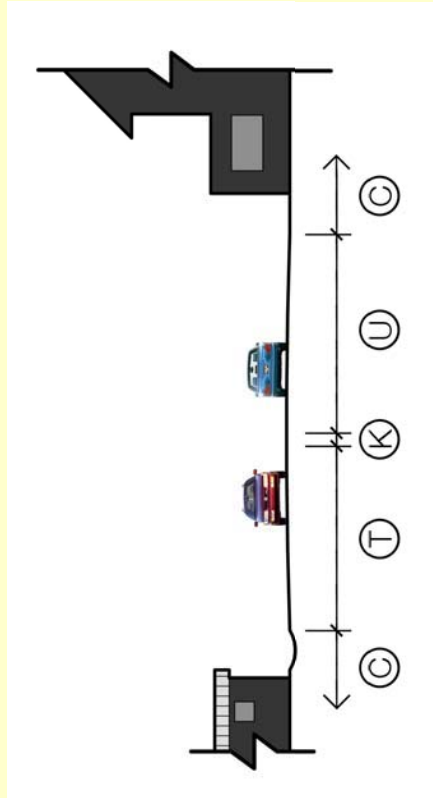
Legend

- A Eastbound Business 80
- B Highway Shoulder
- C Unimproved Roadside Shoulder
- D Westbound Lane
- E Eastbound Lane
- F Unimproved Street and Landscape Setback
- G Curb, Gutter and Enhanced Paving
- H 6' Bike Lane
- I 12' Westbound Lane
- J 12' Eastbound Lane
- K Striped Divider
- L 6' Sidewalk
- M 10' Minimum Landscape Setback
- N 3' Sidewalk
- O Vacant Parcel
- P 2 Westbound Lanes
- Q 2 Eastbound Lanes
- R 10' Landscape
- S 11' Median / Dual Turn Lane
- T Northbound Lane
- U Southbound Lane
- V 12' Northbound Lane
- W 12' Southbound Lane



Design Response

Section D

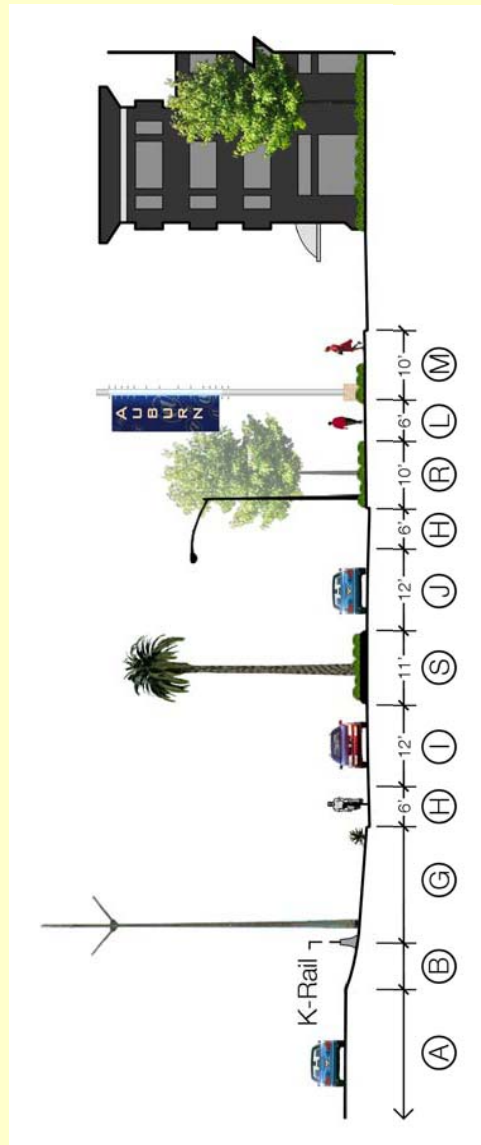
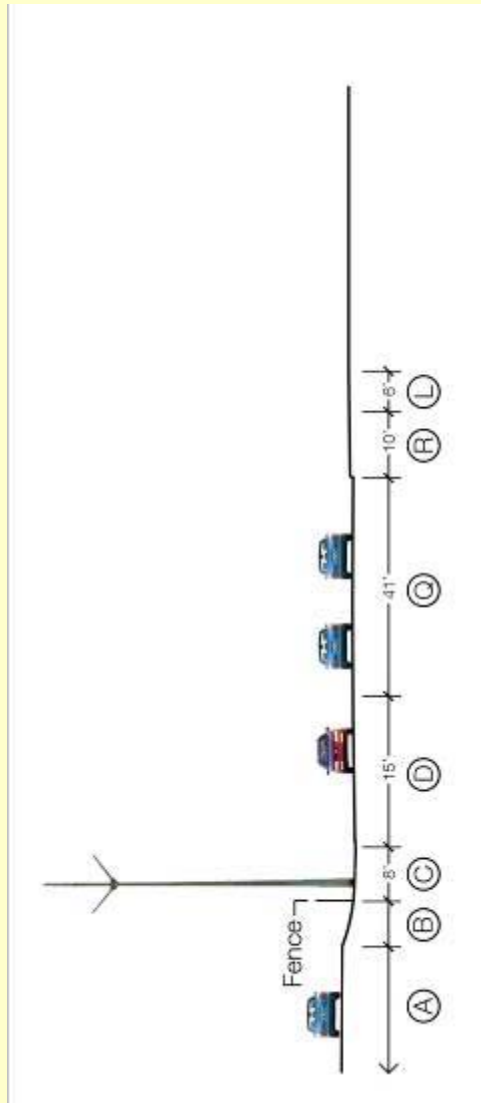


- Legend**
- (A) Eastbound Business 80
 - (B) Highway Shoulder
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 - (W) 12' Southbound Lane



Design Response

Section E



Legend

- (A) Eastbound Business 80
- (B) Highway Shoulder
- (C) Unimproved Roadside Shoulder
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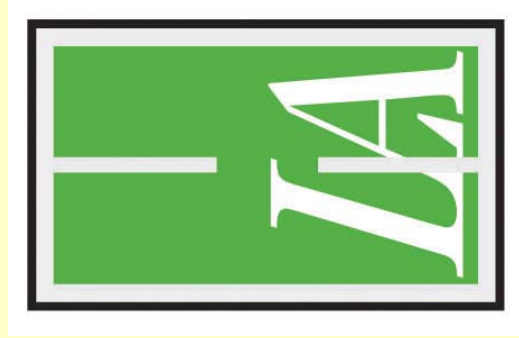
Design Response Perspective 1



Design Response Perspective 2



The vision to anticipate that which may
come to be.



The creativity to realize what
is envisioned.





Auburn Boulevard
 Special Planning Area & Streetscape Master Plan
 County of Sacramento





Auburn Boulevard
Special Planning Area & Streetscape Master Plan
County of Sacramento

The HJA Group Landscape Architects & Planners, Inc.
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Auburn Boulevard
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Auburn Boulevard

Special Planning Area and Streetscape Master Plan

Auburn Boulevard Stakeholders Meeting

March 29, 2006

1:00pm

Agenda

Welcome / Introductions

Review of Progress and Outcome of Public Charrette

Round Table Discussion

Planning Options

Streetscape Options

Next Steps

Adjourn



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MEETING MINUTES

Project Name: Auburn Boulevard Special Planning Area (SPA) & Streetscape Master Plan
Auburn Boulevard Stakeholders (ABS) Meeting

Project Number: 53505.07

Meeting Date: March 29, 2006

Meeting Time:

Attending: See sign in sheet

Recorded By: Susan Collopy

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Summary of Meeting:

Divergent Issues, need resolution: Bike lanes and number of traffic lanes

Review Design Response/Make some decisions.
Shared driveways/Shared rear access. Will not be mandated.
Building height- up to 4 stories (Can we go higher?).

Reaction to higher buildings:

Surprised, developer friendly
What would be the uses?
Bigger Scale, intrigued

Consensus on higher building – OK to go for it. No negative responses at charrette

Setback at Auburn – need a standard front setback if buildings move forward, blocks views of existing buildings - don't plan to move the setback forward unlike current urban planning.

Need to clarify buildings @ rear property line – compatibility issues
One storey only.

Slide w/graph shows no buildings at property line plan view shows buildings @ edge.

Landscaping along the setback- liked, could provide identity for the district.

Bus stop/Median Issues:

Move bus stop to Morse Avenue instead of mid-block. Signalize Morse – it is on a list to be signalized.

Coordinate bus stop locations w/RT.

- Median, land width traffic flow issues
- Medians – trucks need to have access
- Medians are along term goal – strategically place them – Look into 5 years, 10 years down the road? Can't really plan for them right now.

Goal: To reduce the number of driveway cuts.



**Concern that first documents don't show median location, need to be designed later, once street has evolved. Define median.

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Bike lane

Consider 1 wide bike lane with a curb, class 1 bike lane?

Bike Master Plan – Update pending. This would be the place to address the issue of whether or not to have a bike lane on Auburn Blvd.

Consensus: Against the Bike Lane.

Bus Stops

1. Existing bus stop at Desert Industries might move to Morse Ave.
2. Bus turn outs would be desirable.

K-Rail

Main issue to business owners:

- Not a priority
- if want, development to occur, improve visibility from freeway
- accelerate this process, will accelerate development.
- Supervisor Peter's office is trying to get proposed landscape improvements on Caltrans ROW to move forward.
- 5 years out? County & Caltrans are working together to move this forward.

K-Rail costs \$300 LF x 2.1 miles.

Signage

- Locations are not finalized yet
- Need a timeline for when would it happen
- Who would pay for signs?
- Transportation fund
- Grants
- Sup. Peters could provide input

Any signage should say Auburn Blvd. not just Auburn to avoid confusion with the town of Auburn. Business owners want to signage allowed on the building.

Bell Avenue off-ramp

Hazardous, no plans to modify interchange, can't correct, note that it is a problem!
2 lanes east of Fulton, lanes go from 2 down to 1 lane, each side of the street, traffic concerns.

Proposed plans has: 1 lane each way (many concerns), *-Solution is not determined yet.*



Funding for Project:

-Plan first, then review funding options, use plan to help get funding could create TBID

**Aware to aesthetics concerns w/K-Rail

K-Rail – if landscaped on both sides, then less graffiti

Consensus on street monuments and signs? Ok, like signs, the large sign is a bigger priority.

Primary issues that are not resolved are: **concerns about the number of traffic lanes proposed, proposed median locations, and the proposed signage requirements.

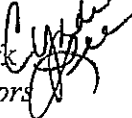
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Approval Document
Conditions of Approval

**COUNTY OF SACRAMENTO
INTER-OFFICE CORRESPONDENCE**

August 28, 2008

TO: PLANNING AND COMMUNITY DEVELOPMENT

FROM: CYNDI LEE, Clerk 
Board of Supervisors

SUBJECT: **06-CZB-ZOB-0695** - (ARDEN ARCADE/BRANDT)
**COMMUNITY PLAN AMENDMENT AND ZONING ORDINANCE
AMENDMENT**
VARIOUS OWNERS - Applicant: County of Sacramento - Located immediately south of the Capital City Freeway, between the Marconi Curve on the west and Watt Avenue on the east, in the Arden Arcade community and the City of Sacramento. (Peters)

The Board of Supervisors, meeting in regular session July 23, 2008 certified the Final Environmental Impact Report as adequate and complete.

The Board of Supervisors, meeting in regular session August 13, 2008, adopted the findings of fact and statement of overriding considerations and took the following actions on the above referenced matter:

COMMUNITY PLAN AMENDMENT

Approved a Community Plan Amendment by Resolution No. **2008-0850** from LC (Limited Commercial), AC (Auto Commercial), GC (General Commercial), TC (Travel Commercial), BP (Business Professional) and RM-1 (Mobilehome Park) to SPA (Special Planning Area) for approximately 108.4 acres, subject to the findings recommended by staff.

REZONE

Approved a Rezone by Ordinance No. **SZC-2008-0023** from LC (Limited Commercial), AC (Auto Commercial), GC (General Commercial), TC (Travel Commercial), Business Professional (BP) and RM-1 (Mobilehome Park) to SPA (Special Planning Area) for approximately 108.4 acres, subject to the findings and conditions recommended by staff.

Zoning Code of Sacramento County
FINAL DOCUMENTS
WEST AUBURN BOULEVARD
06-CZB-ZOB-0695
Various

ZONING ORDINANCE AMENDMENT

Approved a Zoning Ordinance Amendment by Ordinance No. SZC-2008-0023 to add the West Auburn Boulevard Special Planning Area. The SPA also contains the Streetscape Master Plan. The primary goals of the SPA Ordinance and the Streetscape Master Plan are to:

- a. Create a strong, recognizable community identity;
- b. Provide a safer and more effective environment for pedestrians, bicyclists, motorists, and business owners;
- c. Increase visibility to West Auburn Boulevard from passing traffic on the Capital City Freeway;
- d. Promote infill and commercial development by supporting existing businesses with business improvement projects;
- e. Recommend land uses, policies and improvements that will encourage a variety of business ventures and strengthen the competitiveness of the Plan area;
- f. Strengthen urban form by providing an enhanced edge to the Arden-Arcade community; and
- g. Reinvest in retail and service components that service the regional and local communities.

Note: The SPA document also includes property located within the City of Sacramento. This portion of the SPA will be processed separately by the City of Sacramento.

Note: The project will require an override of the County General Plan Comprehensive Land Use Plan (CLUP) Policies to allow the proposed range of uses within the approach/departure safety zone of McClellan Airport. Approval of this project will require a two-thirds vote by the Board of Supervisors.

CL:am

Attachments: Resolution No. 2008-0850
Ordinance No. SZC-2008-0023

cc: In-House
Owner
Applicant

RESOLUTION NO. 2008-0850

**RESOLUTION OF THE BOARD OF SUPERVISORS OF THE
COUNTY OF SACRAMENTO, STATE OF CALIFORNIA,
AMENDING THE ARDEN ARCADE COMMUNITY PLAN
Assessor's Parcel Nos. Various**

WHEREAS, the County of Sacramento has adopted the Arden Arcade Community Plan for the purpose of guiding the County of Sacramento, relative to the development of said community; and

WHEREAS, on June 10, 2008, public hearings have been held before the Arden Arcade Community Council; and

WHEREAS, the Arden Arcade Community Council forwarded its recommendation of approval to this Board.

NOW, THEREFORE, BE IT RESOLVED that the Arden Arcade Community Plan is amended as shown on Exhibit "1" attached to corresponding Rezone Ordinance SZC-2008-0023.

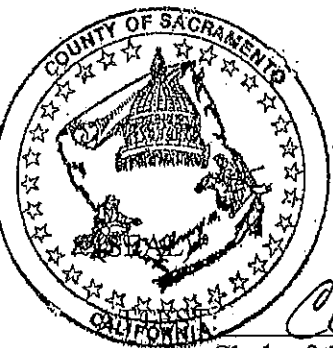
BE IT FURTHER RESOLVED that the Clerk of the Board of Supervisors is directed to endorse said Community Plan to show that such plan has been amended, and that such amendment has been approved by the Board of Supervisors.

On a motion by Supervisor Peters, and seconded by Supervisor Dickinson, the foregoing Resolution was passed and adopted by the Board of Supervisors of the County of Sacramento, State of California, this 13th day of August, 2008, by the following vote, to wit:

- AYES: SUPERVISORS: Dickinson, MacGlashan, Peters, Yee, Nottoli
- NOES: SUPERVISORS: None
- ABSTAIN: SUPERVISORS: None
- ABSENT: SUPERVISORS: None

Jamie R Yee

Chairman of the Board of Supervisors
of Sacramento County, California



Cynthia Lee
Clerk of the Board of Supervisors

In accordance with Section 26108 of the Government Code of the State of California a copy of this document has been delivered to the Chairman of the Board of Supervisors, County of Sacramento on

AUG 13 2008

By *Alison Murphy*
Deputy Clerk, Board of Supervisors

FILED

AUG 13 2008

BOARD OF SUPERVISORS
BY *Cynthia Lee*
CLERK OF THE BOARD

Control No.: 2006-CZB-ZOB-0695
West Auburn Boulevard Special Planning Area

ORDINANCE NO. SZC-2008-0023

**AN ORDINANCE OF THE BOARD OF SUPERVISORS
OF SACRAMENTO COUNTY AMENDING THE
ZONING CODE OF SACRAMENTO COUNTY,
ORDINANCE NO. 83-10, AS AMENDED,
CHANGING THE LAND USE ZONE OF CERTAIN PROPERTY AS SHOWN IN
EXHIBIT "1" OF THIS REPORT**

The Board of Supervisors of the County of Sacramento, State of California, ordains as follows:

SECTION 1: Section 101-116 of the Zoning Code of Sacramento County, Ordinance No. SZC 83-10, as amended, is hereby amended to change the land use zone of the property described in Exhibit "1" (Community Plan Amendment and Rezone) attached hereto and incorporated herein as though set forth in full ("Subject Property").

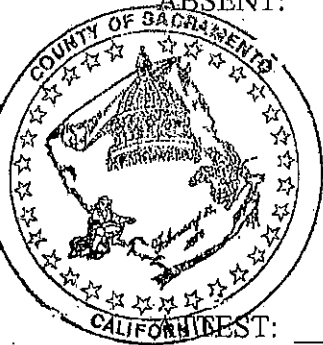
SECTION 2: This ordinance shall take effect and be in full force on and after thirty (30) days from the date of its passage, and before expiration of fifteen (15) days from the date of its passage, it shall be published once with the names of the members of the Board of Supervisors voting for and against the same, said publication to be made in a newspaper of general circulation published within the County of Sacramento, State of California.

On a motion by Supervisor Peters, seconded by Supervisor Dickinson, the foregoing ordinance was passed and adopted by the Board of Supervisors of the County of Sacramento, State of California, this 13th day of August, 2008, by the following vote, to wit:

AYES: Supervisors, Dickinson, MacGlashan, Nottoli, Peters, Yee

NOES: Supervisors, None

ABSENT: Supervisors, None



Juanme R. Yee

CHAIRPERSON, Board Of Supervisors
County of Sacramento, California

Cyrdi Lee

In accordance with Section 26103 of the Government Code of the State of California a copy of this document has been delivered to the Chairman of the Board of Supervisors, County of Sacramento on

AUG 13 2008

By *Alyson Murphy*
Deputy Clerk, Board of Supervisors

FILED

AUG 13 2008

BOARD OF SUPERVISORS
BY *Cyrdi Lee*
CLERK OF THE BOARD

Control No.: 20060695
Type: CZB-ZOB**A D D E N D U M #1**For the Agenda of:
July 23, 2008
Agenda Item No. 9

TO: BOARD OF SUPERVISORS

FROM: PLANNING AND COMMUNITY DEVELOPMENT DEPARTMENT

SUBJECT: **WEST AUBURN BOULEVARD** SPECIAL PLANNING AREA ORDINANCE
AND STREETScape MASTER PLAN

CONTACT: *Troy Givans, Principal Planner, 874-2413, givanst@saccounty.net*
Tricia Stevens, Principal Planner, 874-6141, stevenst@saccounty.net

Overview:

The West Auburn Special Planning Area (SPA) area is within the Arden Arcade Community Special Sign District and the Interstate-Business Route 80 Special Sign Corridor. The SPA Ordinance currently requires that signage in the SPA area conform to the requirements of both the Special Sign District and Corridor, except for specific exemptions listed in *Section Four: "M" (Signs and Graphics)* of the Ordinance. This addendum changes the language to remove the requirement that projects in the SPA area conform to the Special Sign Corridor regulations. Staff believes that these regulations are overly restrictive for this business area. The Special Sign District regulations will provide adequate commercial signage and maintain the requirements for quality monument signage.

In addition, a number of other modifications to the document have been made and are included in the attached Errata Sheet (Attachment "A"). Of note, Table 3.2 (Permitted Land Uses) now includes a footnote regarding uses not permitted in the Neighborhood Connectivity Zones and one that places additional restrictions on noise related to auto uses. In addition, Table 3.2 now specifies uses that would fall under the "General Merchandise" category (J) but would not be permitted. These changes are to the September 2006 Public Review Draft contained within the EIR.

Staff Recommendations:

Staff is recommending **APPROVAL** of the proposed project as revised.

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- A. Recommended Conditions: *These conditions integrate Mitigation Measures from the Draft EIR and will be included as an appendix to the SPA.*
1. Widen the northbound and eastbound approaches and re-stripe the westbound approaches to the Auburn Boulevard/Fulton Avenue intersection to provide the following lane configurations:
 - a. One left-turn lane, two-through lanes, and a right-turn lane on the northbound approach;
 - b. Two left-turn lanes, two-through lanes, and a right-turn lane on the eastbound approach;
 - c. One left-turn lane, two through lanes, and a right-turn lane with overlap phasing on the westbound approach.

Project Fair Share = 75% of project costs. Trigger- When daily traffic volumes on Auburn Boulevard between Bell Avenue and Fulton Avenue exceed 6,580 ADT and exceed 16,760 ADT on Auburn Boulevard between Fulton Avenue and Business 80 Eastbound Off-ramp (Watt Avenue).

2. Install a traffic signal at the intersection of Auburn Boulevard/Morse Avenue. *Project Fair Share = 84% of project costs. Trigger- When daily traffic volumes on Auburn Boulevard between Fulton Avenue and the Business 80 Eastbound Off-ramps (Watt Avenue) exceed 12,900 ADT.*
3. Install a traffic signal, northbound left-turn lane, and a southbound right-turn lane at the intersection of Fulton Avenue/Business 80 westbound ramps. The following measures shall be included in the design of the traffic signal:
 1. Advanced detection shall be placed on the ramp.
 2. Protocols shall be included in the traffic signal to allow for pre-emption of the signal system to clear the ramp when vehicle queues extend to the advanced detectors.

Project Fair Share = 42% of project costs.

4. In order to reduce PM₁₀/dust emissions to less than significant levels, any/all new construction or improvement projects shall implement level three mitigation as defined by the Sacramento Metropolitan Air Quality Management District, which includes the following measures:
 - a. Keep soil moist at all times;

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- b. Maintain two feet of freeboard space on haul trucks;
 - c. Use emulsified diesel or diesel catalysts on applicable heavy-duty diesel construction equipment.
13. To ensure compliance with General Plan Noise Element standards of 65 dB Ldn or less for residential/transient lodging outdoor activity areas and 45 dB Ldn or less for residential/transient lodging interiors, the following measures shall apply:
- a. Any/all common outdoor activity areas shall be designed and attenuated such that after all applied shielding or buffering mechanisms noise levels are 65 dB Ldn or less.
 - b. Any/all new residential/transient lodging construction shall be designed and attenuated such that the interior noise level does not exceed 45 dB Ldn.

The above measures must be accompanied by an acoustical analysis, prepared by a qualified acoustical consultant and verified by the Department of Environmental Review and Assessment, substantiating that the General Plan Noise Element standards cited above will be met.

14. Table 3.2, Footnote #6 shall be added to all applicable uses that may include service bays, automotive/boat/or motorcycle repair facilities, tire shops, machine shops, or where noise producing machinery is utilized. Additionally, the footnote shall be modified slightly to include car wash facilities and following this, the footnote shall be added to the Car Wash use on Table 3.2.
15. In order to minimize nuisance noise from loading dock operations on nearby residential receptors the following measures should be required for new construction or redevelopment in the SPA area:
- Restrict loading dock operations and truck arrivals and departures to the daytime hours.
 - Prohibit engine idling at loading docks.
 - Place noise attenuating barriers and landscaping around loading docks.

Or, if a deviations from the above are requested, then

- Provide an acoustical analysis, completed by a qualified acoustical consultant that demonstrates through noise attenuating measures that the

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General Plan Noise Element standards of 65 dB Ldn or less for outdoor activity areas and 45 dB Ldn or less for interiors is met at nearby residential receivers.

16. Prior to the issuance of any building or grading permits on APN 254-0020-039 (2300 Auburn Boulevard), APN 254-0020-030 (2540 Auburn Boulevard), and APN 254-0060-086 (3000 Auburn Boulevard), applicant shall consult with the Sacramento County Environmental Management Department (EMD), to obtain a site evaluation and to determine the need for a Soil Management Plan or a Health Risk Assessment. If said analyses are required, all site clean-up recommendations, in consultation with EMD, shall be completed prior to the issuance of any building or grading permit, unless EMD approves clearance due to extenuating circumstances.
17. Prior to execution of SMP improvement projects and redevelopment/development projects within the SPA area, the project proponent(s) shall submit an arborist report for the project impact areas when appropriate habitat exists. The report shall include the species, diameter, dripline, and health of the trees, and shall be prepared by an ISA certified arborist. The report shall include an exhibit that shows the trees and their dripline in proximity to the project improvements. The report shall identify any tree that will be removed and quantify the dripline encroachment from project equipment or facilities.
 - a. With the exception of the trees removed and compensated for through Part B below, all healthy native oak trees that are 6 inches dbh or larger on the project site, all portions of adjacent off-site healthy native oak trees that are 6 inches dbh or larger which have driplines that extend onto the project site, and all off-site healthy native oak trees that are 6 inches dbh or larger which may be impacted by utility installation and/or improvements associated with this project, shall be preserved and protected as follows:
 - (1) A circle with a radius measurement from the trunk of the tree to the tip of its longest limb shall constitute the dripline protection area of each tree. Limbs must not be cut back in order to change the dripline. The area beneath the dripline is a critical portion of the root zone and defines the minimum protected area of each tree. Removing limbs that make up the dripline does not change the protected area.
 - (2) Any protected trees on the site that require pruning shall be pruned by a certified arborist prior to the start of construction work. All pruning shall be in accordance with the American National

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Standards Institute (ANSI) A300 pruning standards and the International Society of Arboriculture (ISA) "Tree Pruning Guidelines."

- (3) Prior to initiating construction, temporary protective fencing shall be installed at least one foot outside the driplines of the protected trees within 100 feet of construction related activities, in order to avoid damage to the tree canopies and root systems. Where encroachment occurs, temporary high visibility protective fencing shall be installed a maximum of one foot outside the work areas in order to minimize damage to the tree canopies and root systems.
- (4) Any removal of paving or structures (i.e. demolition) that occurs within the dripline of a protected oak tree shall be done under the direct supervision of a certified arborist. To the maximum extent feasible, demolition work within the dripline protection area of the oak tree shall be performed by hand. If the certified arborist determines that it is not feasible to perform some portion(s) of this work by hand, then the smallest/lightest weight equipment that will adequately perform the demolition work shall be used.
- (5) No signs, ropes, cables (except those which may be installed by a certified arborist to provide limb support) or any other items shall be attached to the protected trees. Small metallic numbering tags for the purpose of preparing tree reports and inventories shall be allowed.
- (6) No vehicles, construction equipment, mobile home/office, supplies, materials or facilities shall be driven, parked, stockpiled or located within the driplines of protected trees.
- (7) No grading (grade cuts or fills) shall be allowed within the driplines of protected trees, except for the minimum required for construction and streetscape improvements.
- (8) Drainage patterns on the site shall not be modified so that water collects or stands within, or is diverted across, the dripline of any protected tree.
- (9) No trenching shall be allowed within the driplines of protected trees. If it is absolutely necessary to install underground utilities within the dripline of a protected tree, the utility line shall be bored and jacked under the supervision of a certified arborist.

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- (10) The construction of impervious surfaces within the driplines of protected trees shall be stringently minimized. When it is absolutely necessary, a piped aeration system per County standard detail shall be installed under the supervision of a certified arborist.
 - (11) All portions of any masonry wall that will encroach into the dripline protection area of any protected tree shall be constructed using grade beam wall panels and posts set no closer than 10 feet on center. Any wrought iron fencing shall be similarly installed, with posts set no closer than 10 feet on center. Posts shall be spaced in such a manner as to maximize the separation between the tree trunks and the posts in order to reduce impacts to the trees.
 - (12) Trunk protection measures, per Sacramento County standards, shall be used for all protected trees where development/construction activity, including installation of any masonry wall and wrought iron fence, occurs within 10 feet of the trunk of a tree.
 - (13) No sprinkler or irrigation system shall be installed in such a manner that sprays water or requires trenching within the driplines of protected trees. An above ground drip irrigation system is recommended.
 - (14) Landscaping beneath oak trees may include non-plant materials such as bark mulch, wood chips, boulders, etc. The only plant species which shall be planted within the driplines of oak trees are those which are tolerant of the natural semi-arid environs of the trees. A list of such drought-tolerant plant species is available at the Department of Environmental Review and Assessment. Limited drip irrigation approximately twice per summer is recommended for the understory plants.
- b. To the maximum extent feasible, all on-site healthy native oak trees shall be protected and preserved. Any substantial (>20%) encroachment and/or removal of native oak trees shall be compensated by planting native oak trees (valley oak/*Quercus lobata*, interior live oak/*Quercus wislizenii*, and blue oak/*Quercus douglasii*), equivalent to the dbh inches lost, based on the ratios listed below, at locations that are authorized by the Department of Environmental Review and Assessment. On-site preservation of native oak trees that are less than 6 inches (<6 inches) dbh, may also be used to meet this compensation requirement. Encroachment of over 20 percent within the dripline radius of native trees will require compensatory mitigation based on the percentage of encroachment

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multiplied by the dbh. Encroachment over 50 percent will require compensation for the entire tree.

Equivalent compensation based on the following ratio is required:

- one preserved native oak tree < 6 inches dbh on-site = 1 inch dbh
- one deepot seedling (40 cubic inches or larger) = 1 inch dbh
- one 15-gallon tree = 1 inch dbh
- one 24-inch box tree = 2 inches dbh
- one 36-inch box tree = 3 inches dbh

Replacement tree planting shall be completed prior to the issuance of building permits or a bond shall be posted by the applicant in order to provide funding for purchase, planting, irrigation, and 3-year maintenance period, should the applicant default on replacement tree mitigation. The bond shall be in an amount equal to the prevailing rate of the County Tree Preservation Fund.

Prior to the approval of Improvement Plans or building permits, a Replacement Oak Tree Planting Plan shall be prepared by a certified arborist or licensed landscape architect and shall be submitted to the Environmental Coordinator for approval. The Replacement Oak Tree Planting Plan(s) shall include the following minimum elements:

- (1) Species, size and locations of all replacement plantings and < 6-inch dbh trees to be preserved;
- (2) Method of irrigation;
- (3) The Sacramento County Standard Tree Planting Detail L-1, including the 10-foot deep boring hole to provide for adequate drainage;
- (4) Planting, irrigation, and maintenance schedules;
- (5) Identification of the maintenance entity and a written agreement with that entity to provide care and irrigation of the trees for a 3-year establishment period, and to replace any of the replacement oak trees which do not survive during that period.

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- (6) Designation of 20-foot root zone radius and landscaping to occur within the radius of oak trees < 6-inches dbh to be preserved on-site.

No replacement tree shall be planted within 15 feet of the driplines of existing oak trees or landmark size trees that are retained on-site, or within 15 feet of a building foundation or swimming pool excavation. The minimum spacing for replacement oak trees shall be 20 feet on-center. Examples of acceptable planting locations are publicly owned lands, common areas, and landscaped frontages (with adequate spacing). Generally unacceptable locations are utility easements (PUE, sewer, storm drains), under overhead utility lines, private yards of single family lots (including front yards), and roadway medians.

Oak trees <6 inches dbh to be retained on-site shall have at least a 20-foot radius suitable root zone. The suitable root zone shall not have impermeable surfaces, turf/lawn, dense plantings, soil compaction, drainage conditions that create ponding, utility easements, or other overstory tree(s) within 20 feet of the tree to be preserved. Trees to be retained shall be determined to be healthy and structurally sound for future growth, by an ISA Certified Arborist subject to Department of Environmental Review and Assessment approval.

If oak tree replacement plantings are demonstrated to the satisfaction of the Environmental Coordinator to be infeasible for any or all trees removed, then compensation shall be through payment into the County Tree Preservation Fund. Payment shall be made at a rate of \$325.00 per dbh inch removed but not otherwise compensated, or at the prevailing rate at the time payment into the fund is made.

18. Install bicycle lanes on Auburn Boulevard along the westbound and eastbound sides of the street and provide an access network that links bike lanes to bike parking facilities.
19. Provide bike racks at a minimum of two locations within each block, equally spaced, and ensure that new developments provide patrons and employees with bike storage facilities.
20. Provide a pedestrian access network that connects redevelopment to all existing or planned pedestrian facilities contiguous within the project site.
21. Provide a pedestrian pathway through all parking areas that clearly defines a pathway between transit facilities and building entrances.

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22. Minimize barriers to pedestrian access and interconnectivity. Eliminate, where feasible, physical barriers such as walls or berms and slopes between residential and non-residential uses within the SPA area that impede bicycle or pedestrian circulation.
23. Construct bus shelters along the Boulevard at all transit stops.
24. Orient projects towards existing transit, bicycle or pedestrian corridors and reduce setback distances to below minimum standards.
25. Orient 75 or more percent of new homes and/or buildings to face either north or south (within 30 degrees of N/S).
26. Replace incandescent and mercury vapor street and parking lot lights with energy efficient alternatives and install new energy efficient alternatives where new street or parking lot lights are required.
27. Require sustainable landscaping such that 75% or more of the species within the landscape plan are considered drought tolerant species.
28. Require the installation of low-flow toilets in all new development and, where feasible, redevelopment along the Boulevard.

III. ATTACHMENT**A. Errata Sheet of Modifications to Auburn SPA Document**

This addendum report was prepared on July 16, 2008.

Newsletters and Public Mailings



ISSUE NO. 1

WINTER ■ 2006

Help Revitalize Auburn Boulevard

You can make a difference in the future of Auburn Boulevard by participating in a community planning process that will create a new look for the corridor.

Once "America's Main Street" and a major highway running from San Francisco to Atlantic City, Highway 40 helped define the golden age of the automobile. Art Deco diners and motels enabled cross country drivers to travel comfortably and conveniently. No longer designated as a highway, Auburn Boulevard today shows only hints of its glory days as Highway 40.

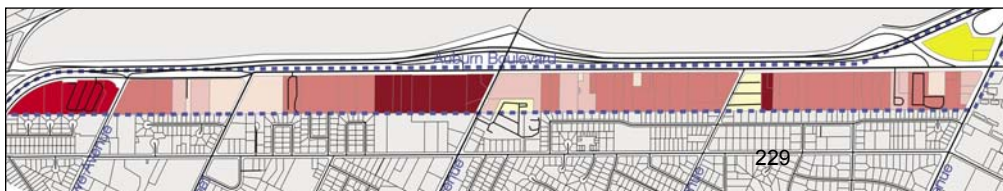
Now the Sacramento Housing & Redevelopment Agency and

the County of Sacramento are launching a planning and design initiative to shape development on Auburn Boulevard. Streetscapes and land use plans will pave the way for improved traffic flow, create an attractive commercial and community identity, and revitalize the corridor's economy.

Residents, businesses and property owners affected by improvements are urged to participate in the planning for Auburn Boulevard by attending the two design charettes described on page 2.

Come join the community planning process and help determine Auburn Boulevard's future.

Project Area



IN THIS

Issue



02 The Process



03 Storied Past, Bright Future



03 Get in Touch



04 Building Community

Community input process works...

Charlotte is held to present conditions and get ideas from residents, businesses and owners.



PLAN AREA ORDINANCE

des land uses, building heights, requirements, recommended rural styles, and other guidelines community input.



community comments, the team creates a final plan and



GET IN TOUCH

In addition to the Auburn Boulevard Streetscape project designed to revitalize the area, the County is undergoing a process of decentralizing many services. The new service model is designed to provide residents and business owners of the Arden Arcade area with additional points of contact within the community and to provide more efficient public services. To coordinate this effort, the County has appointed a Service Area Manager for the Arden Arcade area to assist the community in accessing public services. To help in these efforts, a partial list of important contacts is listed here:

Service Area Manager
Bobbe Dworkis
876-5241

Sheriff Non-Emergency
874-5115

Abandoned Vehicles/Hazard & Nuisance
875-5656

Hazardous Waste Removal
875-5555

Sheriff Service Center
876-5700

Curb, Gutter & Sidewalk Maintenance
875-5171

Graffiti Removal Reporting
875-5151

Sewer Emergencies
875-6730

Storied Past, Bright Future

The Auburn Boulevard Redevelopment Area is a two-mile commercial corridor comprised of 118-acres in the County and City of Sacramento fronting the Capital City freeway. The Redevelopment Area was adopted in October 1992, in response to physical and economic deterioration, and illegal activity. In response to these conditions, the Agency and County successfully partnered with concerned property and business owners, residents, and other agencies to address the pressing health and safety issues.

Redevelopment initiatives tackled a number of the physical challenges impacting Auburn Boulevard. For example:

- A lack of sufficient water flow prohibited all new development on the corridor. In 2001, two miles of 12-inch water line were installed, funded by \$1.2 million of Community Development Block Grant (CDBG) funds;
- A new 67-room Hampton Inn and Suites replaced the old Rolling Green Motel; and

- The former Dodge City Inn was redeveloped into the 146-unit Ladi Senior Apartments.

With renewed private sector investment in new and existing corridor businesses, the future looks bright for Auburn Boulevard. The Streetscape Master Plan and Special Planning Area ordinance are the next critical elements of the corridor's redevelopment.

Combined, they will position Auburn Boulevard to attract new investment and to respond quickly to the changing needs of the business community. Vision, smart planning, strategic investment, and the continued partnership among the County, Agency, business and community leadership are the keys to success – a safe, attractive, competitive Auburn Boulevard.



Article 1



Coming Events

- **Charette #1**, February 9, 2006
Thursday 6-8 PM
Clarion Hotel, Sycamore Room
Fulton Avenue & Auburn Boulevard
2600 Auburn Boulevard
Sacramento, CA 95821
- **Charette #2**, March 23, 2006
Thursday 6-8 PM
- **Community Presentations**
 - Draft Plan, May 2006
 - Approvals, Summer 2006

For More Information call
Troy Givans 874-2413



Building Community

Exciting changes are happening in your neighborhood and you can be part of them. Your fresh ideas can help determine how Auburn Boulevard between Howe and Watt Avenues can grow and improve. Creating a new Specific Plan Area Ordinance and Streetscape Master Plan is intended to:

- **Stimulate job growth**
- **Create a new identity**
- **Beautify the area**
- **Attract a wider variety of businesses**
- **Improve safety**
- **Stimulate Reinvestment**

What are your ideas for Auburn Boulevard? Think of other neighborhoods, other cities—what do you admire about them? What do you like about your neighborhood? What makes it special? What do you want to change the most? What types of

businesses would you like to see?

Two community charettes are scheduled so you can learn more about plans to improve Auburn Boulevard. You are invited to attend these meetings and share your opinions on the current conditions and how to improve them:

February 9, 2006: See maps, photos and hear some history of the area. Then share your ideas about what you like and what you want for Auburn Boulevard.

March 23, 2006: See draft plans for the recreated future of Auburn Boulevard and have a chance to make suggestions for the final plan.

Have your voice heard. Join the process to bring positive change to Auburn Boulevard.

Planning Auburn Boulevard

Join us at a Community Design Workshop



Thursday, February 9, 6-8 PM, 2006
Clarion Hotel, The Sycamore Room
2600 Auburn Boulevard, Sacramento

For more information call Troy Givans, County of Sacramento
Economic Development Department, 916-874-2413

Have your voice heard

Planning Auburn Boulevard

Join us at a Community Design Workshop




Thursday, March 23, 6-8 PM, 2006
Clarion Hotel, Sacramento
2600 Auburn Boulevard/Fulton Avenue

For more information call Troy Givans, County of Sacramento
Economic Development Department, 916-874-2413

Have your voice heard

Sacramento's

Auburn
BOULEVARD

ON U.S.
HIGHWAY 

Thursday

December 15, 2005
1:00 p.m.

Clarion Hotel

2600 Auburn Boulevard
Sacramento, CA 95821

SPECIAL PLANNING AREA AND STREETScape MASTER PLAN

Auburn Boulevard
Stakeholder Meeting

Thursday, December 15, 2005
1 p.m. to 2:30 p.m.


Clarion Hotel

Fulton Avenue & Auburn Boulevard
2600 Auburn Blvd
Sacramento, CA 95821



Sacramento's

Auburn
BOULEVARD

ON U.S.
HIGHWAY 

Thursday

February 2nd, 2006

1:00 PM

**Clarion Hotel
Sycamore Room**

2600 Auburn Boulevard
Sacramento, CA 95821

SPECIAL PLANNING AREA AND STREETScape MASTER PLAN

Auburn Boulevard
Stakeholder Meeting

Thursday, February 2nd, 2006
1:00 PM to 2:30 PM

Clarion Hotel/ Sycamore Room

Fulton Avenue & Auburn Boulevard
2600 Auburn Boulevard
Sacramento, CA 95821

Auburn Boulevard
c/o The HLA Group
1990 Third Street, Suite 500
Sacramento, CA 95814





H a v e Y o u r V o i c e H e a r d

PLANNING AUBURN BOULEVARD

Join Us at a Community Design Workshop

Auburn Boulevard
Community Design Workshop

Auburn Boulevard
c/o The HLA Group
1990 Third Street, Suite 500
Sacramento, CA 95814



Thursday, March 23, 6-8 PM, 2006
Clarion Hotel, Sacramento
Fulton Avenue & Auburn Boulevard
2600 Auburn Boulevard

For more information contact Troy Givans,
County of Sacramento Economic
Development Department, 916-874-2413